

City of Mt. Angel

December 15, 2016

Planning Commission Packet

MT. ANGEL PLANNING COMMISSION MEETING AGENDA
290 East Charles Street – Community Meeting Room
Thursday, December 15, 2016, 7:00 p.m.

7:00 p.m. REGULAR MEETING

- 1. Call to Order**
- 2. Flag Salute**
- 3. Roll Call**
- 4. Approval of April 21, 2016 Meeting Minutes**
- 5. Public Hearing on LA 2016-01 - Planning Commission review of proposed amendments to the Mt. Angel Development Code.**
- 6. Other Business**
- 7. Adjournment**

Mt. Angel Community Meeting Room is handicapped accessible. If special accommodations are required please contact City Hall at least one business day in advance at 503-845-9291. Hearing Impaired may call TTY (800) 648—3458. Interpretive services may be available with sufficient prior notice of need.

City of Mt. Angel
Planning Commission Meeting Minutes

Planning Commission

7:00 PM

April 21, 2016

The City of Mt. Angel's Planning Commission met Thursday, July 16, 2015 at the Community Meeting Room, 290 E. Charles Street, Mt. Angel, Oregon.

1. CALL TO ORDER:

The meeting was called to order by Commissioner Wall at 7:00 p.m.

2. FLAG SALUTE:

Commissioner Wall led the salute to the flag.

3. ROLL CALL:

PLANNING COMMISSION

Craig Emch, Commissioner
 Ryan Kleinschmit, Commissioner & Chair
 Greg Savage, Commissioner
 David Sylvia, Commissioner
 Jeff Wall, Commissioner & Vice Chair

STAFF

Justin Hogue, Assistant to the City Manager
 McRae Carmichael, City Planner (Mid-Willamette Valley Council of Governments)
 Renata Wakeley, (Mid-Willamette Valley Council of Governments)

4. APPROVAL OF JULY 18, 2015 MINUTES

Commissioner Savage requested that his comments on potential change of use be reflected in the July 16, 2015 minutes.

Commissioner Emch moved to approve the Planning Commission meeting minutes for July 16, 2015 as amended; Commissioner Wall seconded.

Craig Commissioner Emch: **Aye**
 Commissioner Kleinschmit: **Aye**
 Commissioner Savage: **Aye**

Commissioner Sylvia: **Aye**
 Commissioner Wall: **Aye**

Ayes: 5 Nays: 0

The motion passed unanimously.

5. PUBLIC HEARING

a. Design Review 2016-01/ Variance 2016-01: Subject Property 555 N Main Street, Map Tax Lot 061W030CD/1700

Commissioner Kleinschmit explained the public hearing procedures and asked if there were any conflicts of interest, biases, or ex parte contact. Commissioner Emch declared that he had lunch with the Mt. Angel Fire Department Chief and the land use application did come up during their

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lunch, but there was nothing of material in their discussion and does not hold any bias as a result of that conversation. There were no other declarations of ex parte by the Commissioners. Commissioner Kleinschmit also asked the audience if they were conscious of any possible conflicts of interest, biases, or ex parte contact by the Commissioners. None were provided by the audience. Commissioner Kleinschmit provided the opening statement required under state law prior to opening the public hearing.

Commissioner Kleinschmit opened the public hearing at 7:09 p.m.

Commissioner Kleinschmit requested that the staff report be presented

McRae Carmichael, City Planner, provided the staff report. She spoke of zoning for the subject lot and lots located in its' immediate vicinity. She spoke of the criteria for a variance in Mt. Angel Development Code (MADC) 4.4(a). She explained that a triplex is allowed in the Multi-Family Zone and that the applicant is requesting a variance for the side yard setback due to the subject property being a narrow, long lot. She presented the findings in the staff report. She spoke of the shape of the lot and the complex conditions it creates for development and how the applicant is attempting to maximize use of the lot. The applicant is proposing a variance to the setback and they are meeting all other requirements.

She resumed her presentation of findings listed in the staff report. She also commented that the application does not conflict with the current zoning designation or comprehensive plan. The property is remaining consistent with master plan, comp plan, and design standards. The use does not require that the property be rezoned. The applicant will not be minimizing the property by any means and does not need to reclassify the rezoning. She remarked that the applicant is looking to develop the property according to the permitted use.

She concluded by stating granting the variance will not limit the other properties to build out and develop as desired. She stated that all of the criteria has been met. The Commission is being asked to consider the application to vary the southern property line setback. She explained the options available to the Commission.

Commissioner Kleinschmit asked how the Code determines what is considered the side yard versus the back yard. City Planner McRae described how the code defines the yards. She spoke about the requirement in the infill overlay zone and how a façade is required for the front lot line. For purposes of the Code the southern lot line is the side yard lot line. She stated functionally that is it the rear yard. Commissioner Savage asked about orientation of the buildings as it relates to the back yard. City Planner McRae described the building orientation.

Brandie Dolton introduced herself as the representative for the applicant. She described some of the conditions of the property. She stated the side of building will face the street. She said the triplex does meet parking requirements and parking is allowed on Main Street. The traffic generated is not enough for a traffic impact analysis. There is a 16 foot access way and the applicant is going to extend that to 20 feet. Commissioner Sylvia asked where the parking would be. Commissioner Kleinschmit asked about the ability to meet the fire lane requirement. Commissioner Emch asked about the measurement of the setback on the east end of the property.

Commissioner Kleinschmit asked about the plan to keep people from parking in the fire lane. Ms. Dolton stated that with other similar developments, enforcement can be difficult and tenant awareness is key. Commissioner Sylvia asked if you could still meet the parking requirements. Ms. Dolton stated that would not affect parking.

Commissioner Savage asked about the Fire Code. Commissioner Kleinschmit asked if there were any more questions for the applicant. Commissioner Sylvia asked about the fence, commenting that it wasn't indicated in the applicant's submittal. Commissioner Emch asked about engineering and other designs. Ms. Dolton presented the commission with the original proposal. She stated that staff at the time did not think they could support the original proposal.

Commissioner Kleinschmit asked if there were any more questions. Commissioner Kleinschmit asked if there was anyone that was in favor, neutral, or anyone one in opposition who wished to speak to the application.

Ms. Dolton stated that the original design would not have allowed the applicant to meet the parking requirements. City Planner McRae spoke to the infill overlay and how it prevents parking from occurring in the front yard. Commissioner Kleinschmit stated we are taking the practicality of it being a back yard when it is only 5 feet deep and asked about distance from the rear porch. Mr. Lazar, the applicant, stated the 5 feet is measured from the building foundation. Commissioner Kleinschmit asked if there were any other questions for the applicant.

Commissioner Emch asked if any thought about egress for emergency vehicles. There was a question about emergency access. City Planner McRae stated the Building Code and Fire Code will govern those issues. Commissioner Savage asked about the possibility a smaller foot print and more working space. City Planner McRae stated this is not outlined in the proposal before the Commission. Commissioner Savage asked about comments from the neighboring properties. City Planner McRae stated the properties received notice of the hearing.

Commissioner Kleinschmit closed the public hearing at 7:42 p.m.

Commissioner Sylvia stated that it is really up to those that want to live there. Commissioner Savage asked why a duplex was not looked at. Commissioner Wall thinks it is too much of a change. Commissioner Emch stated that it is a good design and that it is a vacant lot currently. City Planner McRae stated the question is whether the criteria has been met, if it has not, then state how the criteria has not been met. Commissioner Savage spoke about the findings such as the effect on health and safety and parking. Commissioner Kleinschmit stated that there are three separate households and would like clarification on building orientation.

Commissioner Kleinschmit was concerned with meeting the Code and understanding how one unit facing the street meets the Code. It was reviewed as one unit with the main entrance allowing it to meet the code. There was discussion on backyard setbacks. Commissioner Kleinschmit asked if anyone wanted to entertain a motion.

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**CITY OF MT. ANGEL
PLANNING COMMISSION HEARING
STAFF REPORT**

REPORT DATE: December 6, 2016

FILE NUMBER: LA 2016-01

HEARING DATE: December 15, 2016

SUBJECT: Planning Commission review of proposed amendments to the Mt. Angel Development Code.

CRITERIA: Oregon Statewide Planning Goals and Guidelines
City of Mt. Angel Comprehensive Plan
City of Mt. Angel Development Code

EXHIBITS: Exhibit A: Draft Text Amendments
Exhibit B: Design Charrette Report
Exhibit C: Stakeholder Summary

BACKGROUND:

The City requested and received a code assistance grant from the Transportation and Growth Management (TGM) Program. Through this grant, the City completed a code assessment (Phase I) in order to create an action plan to guide code amendments in Phase II of the Project. The goal of the Phase II TGM Code Assistance project¹ was to create recommendations for updating Mt. Angel's Development Code in order to help the city enhance its downtown area, strengthen regulations that control downtown development, and improve efficiency in the land use process. Specifically, identified objectives for this project are:

- Emphasize the pedestrian environment, particularly in the downtown core, to strengthen the Bavarian theme and ensure future projects will be consistent with, and enhance downtown development.
- Improve the multi-modal nature of the downtown area by providing safe and convenient access for walking and biking.
- Identify areas where revisions may be needed to improve the overall function, reliability and user-friendliness of the Development Code. *Note: this objective was identified as a priority at the City Council-Planning Commission work session on July 16, 2015.*

¹*This project is partially funded by a grant from the Transportation and Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Moving Ahead for Progress in the 21st Century (MAP-21), local government, and the State of Oregon funds. The contents of this document do not necessarily reflect views or policies of the State of Oregon.*

SUMMARY OF PROPOSED AMENDMENTS:**1. Section 1.10 Definitions, Subsection (b) Definitions**

The proposed amendment adds definitions for “Dwelling, Attached Single Family” and “Façade”, alters the definition for “Accessory Structure” to remove language regarding the “portion of a main building,” and removes a redundant definition for “Clear Vision Area” by merging it with the definition for “Vision Clearance Area.” The proposed changes will add clarity and improve the overall function and user-friendliness of the code.

2. Section 2.2 General Provisions, Subsection (e) Quasi-Judicial Actions

The proposed amendments alter language regarding Type II and Type III land use actions in order to reduce conflicts with subsequent sections and improve the clarity and overall function of the code.

a. Subsection (e)(1)

The proposed amendment adds language requiring a pre-application conference with city staff prior to the submittal of a land use application for Type II or Type III actions in order to help streamline the review process for staff and applicants.

b. Subsection (e)(2)

The proposed amendment extends the length of time given to staff to notify the applicant of missing information in the application to 30 days from the receipt of the application, consistent with the requirements for Type I actions. If the applicant refuses or fails to submit the missing information, the length of time to which the application will be deemed complete for the purposes of a land use action is extended to 180 days after the original submission.

c. Subsection (e)(4)

The proposed amendment removes the language regarding notice of a public hearing for Type II or Type III actions, and directs the user to the requirements in Section 2.3 of the code.

d. Subsection (e)(8)

The proposed amendment reduces the number of days within which the city is required to mail the notice of decision to 12 days from the date of action. In addition, language is added to require any group or participant who submitted oral or written public testimony for the hearing or requested notice of the decision be mailed notice of decision, and removes language requiring notice for adjacent property owners within 100 feet of the subject property.

3. Section 2.2 General Provisions, Subsection (g) Pre-Application Conferences

The proposed amendment adds a new section of code outlining the purpose, applicability, and requirements for pre-application conferences.

4. Section 2.3 Notification of Hearing, Subsection (a) Quasi-Judicial Hearings (Type II and Type III)

The proposed amendment revises language regarding requirements for notice of public hearings to ensure clarity and consistency with prior sections of code.

a. Subsection (a)(1)

The proposed amendment removes language requiring notice to be published in a newspaper prior to the hearing. It also amends the requirement that property owners within 100 feet of the subject property be mailed notice to state that the failure of owners to receive notice by mail will not affect the proceedings of the hearing, and adds new language requiring notice of hearings be mailed to any person who submits a written request to receive notice, as well as any governmental agency entitled to notice under a relevant intergovernmental agreement.

b. Subsection (a)(2)

The proposed amendment adds language requiring the applicant to post notice of the hearing on the project site in a visible location using a format prescribed from the City Planning Official at least 14 days prior to the date of the first hearing.

c. Subsection (a)(3)

The proposed amendment adds language requiring the city to publish notice of the hearing on the city website and/or in a local newspaper at least 14 days prior to the first hearing.

5. Section 2.3 Notification of Hearing, Subsection (b) Legislative Hearings (Type IV)

The proposed amendment reduces the number of days from the hearing at which the Planning Commission or City Council is required to provide notice to the Department of Land Conservation and Development (DLCD) to at least 35 days which is consistent with recent changes to State requirements.

6. Section 5.10 Automatic Conditional Use Status

The proposed amendment adds Section 5.10, regarding Automatic Conditional Use Status, to Section 5 of the code. The proposed language ensures that, in the event of a change to zoning regulations for a specific site, any preexisting uses that were either nonconforming or allowed by right, and which are now listed as conditional uses, shall be considered approved conditional uses and may continue to operate. The language also notes that any changes to the use will be subject to the requirements and approval criteria of Section 5 of the code.

7. Section 6.1 Residential Zone, Single Family (RS), Subsection (b) Permitted Uses

The proposed amendment clarifies language in Subsections (b)(3), (c)(2) and (d)(1) that the difference between permitted, transitional and conditional “public buildings and structures” in the Single Family Residential zone is based on building height: 35 feet or less is a permitted or transitional use, greater than 35 feet is a conditional use.

8. Section 6.1 Residential Zone, Single Family (RS), Subsection (e) Dimensional Standards

The proposed amendments revise the minimum requirements for development in RS zones.

a. Subsection (e)(1)

The proposed amendment removes language in Subsection (1)(C) allowing the creation of vacant parcels with a minimum size of 5,600 square feet, and adds language referencing the Infill Overlay Zone.

b. Subsection (e)(2)

The proposed amendment removes language requiring accessory structures to conform with minimum yard setback requirements in Section 6.1(e), and adds language subjecting projections into setbacks to requirements in Section 7.1 and subjects accessory structures to requirements in Section 7.2.

c. Subsection (e)(4)

The proposed amendment revises Section (e)(4) to improve the clarity and user-friendliness of the code, and also removes the standard for corner lot widths.

9. Section 6.1 Residential Zone, Single Family (RS), Subsection (f) Single Family Dwelling Features

The proposed amendment removes language regarding standards for single-family dwelling features and directs the user to the design standards for new single-family homes and duplexes in Section 7.4 of the code.

10. Section 6.2 Residential Zone, Multi-Family (RM), Subsection (b) Permitted Uses

To remove a conflict, the proposed amendment adds an exception to uses permitted in RM zones for uses listed as conditional in Section 6.2(c) of the code, and removes the specified exception for Bed and Breakfasts. It also adds language specifying attached single-family dwellings (townhouses) as a permitted use in RM zones.

11. Section 6.2 Residential Zone, Multi-Family (RM), Subsection (d) Dimensional Standards

The proposed amendments add language allowing lot sizes to be reduced by up to 20% subject to standards in Section 7.4(d), add the requirement of a minimum of 3,500 square feet for attached single-family dwellings (townhomes) in RM zones, add language subjecting projections into setbacks to requirements in Section 7.1 and subjecting accessory structures to requirements in Section 7.2, and permit the setback for the common wall of an attached single-family dwelling to be reduced to zero. They also remove the standard for corner lot widths, and add a minimum lot width of 24 feet for interior lot width at the building line for townhomes.

12. Section 6.3 Residential Commercial Zone (RC), Subsection (b) Permitted Uses

The proposed amendment adds an exception to uses permitted in RC zones for uses listed as conditional in Section 6.3(c) of the code, and removes the specified exception for Bed and Breakfasts.

13. Section 6.3 Residential Commercial Zone (RC), Subsection (d) Dimensional Standards

The proposed amendment adds language subjecting residential developments and accessory structures in RC zones to the setback requirements of the RM zone. It also revises several of the standards for clarity including establishing a building height in the RC zone consistent with that of the RM zone.

14. Section 6.4 General Commercial Zone (CG)

The proposed amendments revise standards for improved clarity and user-friendliness and to remove redundancies in the code. Language is added to Section 6.4(d) subjecting projections into setbacks to requirements in Section 7.1 and subjecting accessory structures used in connection with residential purposes to requirements in Section 7.2.

15. Section 6.5 Light Industrial Zone (IL)

The proposed amendments revise standards for improved clarity and user-friendliness and to remove redundancies in the code.

16. Section 6.7 Downtown Core Zone Overlay District (DC), Subsection (a) Purpose

The proposed amendment adds language noting that standards of the underlying zone apply to the overlay except as specified in Section 6.7 of the code.

17. Section 6.7 Downtown Core Zone Overlay District (DC), Subsection (b) Permitted Uses

The proposed amendment revises language for the purpose of improved clarity and user-friendliness of the code.

18. Section 6.7 Downtown Core Zone Overlay District (DC), Subsection (c) Dimensional Standards

The proposed amendment adds language subjecting projections into setbacks to requirements in Section 7.1 and subjecting accessory structures used in connection with residential purposes to requirements in Section 7.2.

19. Section 6.7 Downtown Core Zone Overlay District (DC), Subsection (d) Additional Development and Design Standards

The proposed amendment adds Section 6.7(d), providing standards intended to enhance the pedestrian environment of the downtown, provide civic gathering space, support natural surveillance of public space, and encourage human-scale design. The amendment includes standards for applicability, minimum and maximum setback requirements, Site Design Review, and a variety of design features including: primary entrances, windows and doors, and rooftop equipment and screening.

20. Section 7.1 Yard and Lot Standards

The proposed amendment clarifies language regarding yard setbacks to eliminate conflicts with setbacks established in individual zones.

21. Section 7.2 Accessory Structures

The proposed amendments revise language regarding yard setback standards for accessory structures to clarify which standards should apply and remove conflicts with setbacks established in individual zones. Language is also added subjecting fences and walls associated with single-family development to the standards in Section 7.4 of the code.

22. Section 7.4 Single Family Design Standards

The proposed amendment creates Section 7.4 of the code, which provides standards intended to ensure that new single-family development is compatible with and enhances the existing character of neighborhoods. This section replaces the City's existing standards for single family dwellings in 14.6. The amendment includes design standards addressing the review process, building orientation, a menu of design elements, fences and walls, and development patterns.

23. Section 7.5 Attached Single Family Dwelling (Townhouse) Design Standards

The proposed amendment creates Section 7.5 of the code, which provides standards intended to ensure that attached single-family development is compatible with and enhances the existing character of neighborhoods. The amendments include standards for the location, lot size, building setback, number of units, open space requirements, and design. The amendment language incorporates concepts from Sections 14.6 and 14.7 from the Infill Development Overlay Zone, which are proposed to be deleted.

24. Section 7.6 Multifamily Design Standards

The proposed amendment creates Section 7.6 of the code, which provides standards intended to ensure that new single-family development is compatible with and enhances the existing

character of neighborhoods. The amendments include standards for offsets and variation, facades, main entrances, parking lots and landscaping, and screening. The amendment language incorporates concepts from Section 14.7 from the Infill Development Overlay Zone, which is proposed to be deleted.

25. Section 7.7 Pedestrian Connectivity Standards

The proposed amendment creates Section 7.7 of the code, which provides standards intended to provide for a safe, direct, and convenient pedestrian walkway system, and supports and complements building design standards for the downtown core. The amendment includes requirements for applicability, as well as standards for developing a walkway system that is continuous, safe, direct, and convenient with separation between the walkway and street, and constructed of a durable surface with adequate width to accommodate pedestrian movement.

26. Section 8.3 Reduction of Parking and Loading Area Allowed in the Downtown Core

The proposed amendment removes language referencing the Parking District, and replaces it with language referencing the Downtown Core Zone Overlay District thereby expanding the coverage of the Parking District. It also adds requirements for parking to be provided for new residential developments, and new non-residential buildings with a floor area of over 4,000 square feet or more than 10 employees.

27. Section 8.6 Off-Street Parking

The proposed amendment revises language for clarity.

28. Section 11.6 Permit Requirements

The proposed amendment removes the requirement to include a statement of valuation of the signs in the application for a sign permit.

29. Section 11.7 Design Review Requirements

The proposed amendment includes an extended list of approved styles of lettering for signs, and replaces references to the title of Design Review Committee with Design Review Board.

30. Section 11.13 Special Category Signs

The proposed amendment replaces references to the title of Design Review Committee with Design Review Board for consistency.

31. Section 14 Infill Development Overlay Zone

The proposed amendment deletes Sections 14.6 (Residential Development Standards) and 14.7 (Additional Multi-Family Residential Development Standards). These sections have been replaced by 7.4, 7.5 and 7.6.

32. Section 16.1 Purpose of the Design Review Board

The proposed amendment notes that the Board serves as an advisory board to City staff or the Planning Commission in order to clarify its role and purpose as an advisory body, not a decision-making authority.

33. Section 16.4 Review Authority

The proposed amendment removes language suggesting that the Design Review Board should review single-family dwelling projects.

34. Section 16.5 Procedures

The proposed amendment clarifies the type of review for which the Board advises either the City staff planner or the Planning Commission.

35. Section 16.6 Appeals

The proposed amendment removes the language regarding appeals in order to help clarify the role of the Board as an advisory body rather than a decision-making body.

FACTS AND FINDINGS:

Procedural Findings:

- A. In response to the 1973 State Land Use Act, the City of Mt. Angel embarked on a program to update its 1966 Comprehensive Plan, a process which included extensive public input and the formation of a Committee for Citizen Involvement. The resulting revision Plan was formally adopted by Ordinance 468 on September 12, 1977 and was acknowledge by the Department of Land Conservation and Development (DLCD) on July 25, 1978.
- B. In June of 1981 the City completed an update of the Comprehensive Plan that added, deleted, or changed the main document and was adopted as an element of the original document, which did not supersede the 1977 edition.
- C. On September 30, 1986, the DLCD notified the City that it must make a periodic review of the Comprehensive Plan to keep it current with changes in the community and statewide planning goals. The updated document, completed in June of 1987, was officially adopted in May of 1988, by Ordinance 551.
- D. Mt. Angel has undertaken a number of plans for the City's downtown, including the ODDA Downtown Resources Team Report in 1999, the Mt. Angel Downtown Plan in 2001, an update to the Transportation System Plan in 2003, and a Transportation Growth Management (TGM) Workshop Report in 2008. Each of the aforementioned plans explores and supports TGM and smart development principles.
- E. In 2014, the City of Mt. Angel was the subject of a University of Oregon "Green Cities" class, which included holding a Design Charrette Community Workshop and the production of a Planning and Design Report focused on the City's Bavarian themed district.
- F. In 2015, the City received TGM funds to contract with a consultant to create an action plan for future code changes that support TGM objectives and smart development practices. The program was kicked off in July of 2015.
- G. A Joint Planning Commission and City Council Work Session was held on July 16, 2015 to discuss the City's expectations and ideas and gather information.

- H. In September 2015, stakeholder interviews and a Planning Commission Work Session were conducted to obtain input on Phase I of the TGM Code Assistance project.
- I. A second Joint Planning Commission and City Council Work Session was held on December 14, 2015 to review the draft action plan prepared by the consultant and make recommendations for the Final Action Plan Memo. This memo was used as the basis for Phase II of the Code Assistance project.
- J. In 2016, the City secured funding to amend the initial contract to include the preparation of amendments to the City's development code consistent with recommendations made in the action plan, presentation of the recommended amendments at a Joint City Council and Planning Commission Work Session and subsequent City Council and Planning Commission Public Hearings, and preparation of the final text amendments to the development code following adoption by the City Council.
- K. Two Joint Planning Commission and City Council Work Sessions were held in September of 2016 to discuss the draft code amendments.

Substantive Findings:

1. APPLICABLE COMPREHENSIVE PLAN GOALS AND POLICIES

1.1. Land Use Planning

Goals:

(1) *To ensure that the Mt. Angel Comprehensive Plan reflects the desires and needs of city residents.*

Objectives:

(1) *Provide the opportunity for continual public input to local decision making.*

(2) *Periodically review the comprehensive plan to ensure that the plan continues to reflect the wishes and needs of the Community.*

Policies:

(1) *Maintain an active citizen involvement program to provide for continued public input into planning decision making.*

Findings:

The officially adopted 1977 and 1987 editions of the City's Comprehensive Plan involved extensive public engagement and input from the Committee for Citizen Involvement. Consistent with Land Use Planning Goal 1, Objectives 1 and 2, and Policy 1, the proposed amendments incorporate input from the public gathered throughout the process and from the Design Charrette conducted in 2014. The amendments also incorporate input received during the stakeholder interview process that occurred in Phase 1 of the Code Assistance project.

1.2. Residential Land Use and Housing

Goals:

(1) *To assure that residential areas are pleasant, healthful and safe places in which to live.*

(2) *To assure that all existing and future residents of the community have the opportunity to acquire safe and sanitary housing affordable to all income and age groups.*

(3) *To maintain, conserve and enhance housing units in the city.*

Objectives:

(1) Encourage the use of new land development and design techniques to allow for a variety of living areas and housing types, and which promote the development of odd-shaped lots or underutilized residential land, and which may bring about significant cost reductions to housing.

(7) To assure continued enforcement and compliance with the building, housing and zoning codes.

Policies:

(3) High density developments should have good access to arterial and collector streets and be located close to employment and shopping areas.

(5) Encourage development of housing which meets the needs of all income levels of existing and future residents of the city.

(6) Encourage a mixture of low and moderate cost housing to avoid an undue concentration in anyone area.

Findings:

Consistent with Residential Land Use and Housing Goals 1 through 3, Objectives 1 and 7, and Policies 3, 5, and 6, the proposed amendments establish a new section of clear and objective residential design standards that will apply city wide to ensure quality housing at reasonable costs, without placing undue burden on the developer. The amendments include design standards for single-family, duplex, and multifamily residential developments intended to enhance neighborhood appearance through the creation of attractively designed housing. The amendments are also intended to simplify the review process for single-family developments by establishing clear and objective standards that can be quickly and easily reviewed by City planning staff.

1.3. Commercial Land Use

Goals:

(1) Maintain the existing business center as the dominant area of commercial activity.

(2) To develop a business center that is easily accessible, convenient and a pleasant place in which to shop.

Policies:

(1) Maintain and enhance the business center as the commercial and community activity center.

(4) New or expanding businesses are to provide off-street parking, and landscaping that is complementary to the area.

(5) To promote energy conservation and to lessen future traffic and parking congestion, pedestrian and bicycle travel should be emphasized through provision of adequately surfaced sidewalks, sloped curb crossings and bicycle racks.

Findings:

The proposed addition of a set of new design standards for downtown development is consistent with and directly applicable to Commercial Land Use Goals 1 and 2 and Policies 1, 4, and 5. The standards are intended to ensure that new development will have a pedestrian-friendly character consistent with the historic and walkable environment that defines downtown Mt. Angel. When the City of Mt. Angel initially secured funding for the Code Assistance project in 2015, the specified project objectives included emphasizing the pedestrian environment of the downtown core, strengthening the Bavarian theme to ensure future development will be consistent with and enhance the downtown core, and improving the safety and convenience of multi-modal access in the downtown core. In addition, the integration of policies from the Parking District into the Downtown Core Overlay District, and proposed removal of the Parking District from the code, is

directly applicable to and improves consistency with Commercial Land Use Policies 4 and 5, and will help to further streamline and clarify the code.

1.4. Industrial Land Use

Goals:

- (1) *To maintain and encourage the development of a sound economic base having a diversity of industry, retail and wholesale establishments and service related industries.*

Findings:

The proposed amendments revise Section 6.5, relating to standards for Light Industrial (IL) zones to improve the clarity and user-friendliness of the code and ensure consistency with Industrial Land Use Goal 1 of the City's Comprehensive Plan.

1.5. Historic Properties

Goals:

- (1) *Provide the Mt. Angel community with a reasonable opportunity to have input into the preservation of structures, sites and areas within the urban area that are of historic interest.*

Objectives:

- (2) *Provide the community and individual property owner the maximum possible opportunity to economically and socially benefit from the existence, continuation and preservation of the historic sites and structures in the community.*

Policies:

- (1) *The City of Mt. Angel recognizes that historical and architectural resources constitute a valuable community resource that contributes to the social cohesion of the community; contributes to a livable environment; and can and does significantly benefit the economy of the community by making it an area of state and regional interest.*
- (10) *It shall be the policy of the City of Mt. Angel to in all public activities preserve the general view and visual quality of those outstanding views and sites inventoried by the comprehensive plan inventory of scenic views and sites included in this plan as "Appendix B".*
- (11) *In the approval of new developments the city shall consider the effect on the general view and visual quality of inventoried scenic views and sites.*
- (12) *Strategies, such as street alignment to "point" toward inventoried scenic views and sites, placement of structures, and landscaping shall be used.*

Findings:

Consistent with Historic Properties Goal 1, Objective 2, and Policies 1, 10, 11, and 12, the proposed design standards for downtown development will ensure that new development is consistent with the historic character that defines downtown Mt. Angel. In addition, standards for new residential development require that in order to encourage the preservation and reuse of historic buildings, no parking shall be required for new or existing multifamily units above the ground floor in buildings built before 1950.

1.6. Sanitary Sewer, Water and Storm Drainage Systems

Goals:

- (1) *To provide and develop a timely, orderly and efficient arrangement of community facilities and services to serve as a framework for urban and rural development.*

Findings:

New standards added to Section 7 are intended, in part, to minimize paved surfaces for better storm water management and are therefore indirectly applicable to Sanitary Sewer, Water and Storm Drainage Systems Goal 1.

1.7. Transportation Element

Goals:

(1) *To provide and encourage a safe, convenient and economic transportation system to serve the needs of the citizens of Mt. Angel.*

Objectives:

(5) *Utilize setback or design techniques to protect established rights-of-way along arterial and collector streets.*

Policies:

(1) *Maintain and upgrade the overall transportation system within the City to meet present and future needs.*

(2) *The design of new roads, streets and thoroughfares shall preserve and enhance natural and scenic resources.*

(3) *The City shall take full advantage of public investment in the existing streets. New streets shall be developed to improve traffic circulation and relieve traffic volume on existing streets.*

(20) *Mt. Angel supports the use of bicycles as a viable mode of alternative transportation and will continue to support the existence and maintenance of a statewide bike lane on Highway 214.*

Findings:

Consistent with the goals, objectives, and policies identified in the Transportation Element of the City's comprehensive plan, improvement of the multi-modal nature of the downtown area by providing safe and convenient access for walking and biking was identified as one of the key objectives for the Code Assistance project. The proposed addition of Section 6.7(d) to the code would provide guidelines for enhancing the pedestrian environment of the downtown area through the use of human-scaled design, as well as setback, entranceway, and street security requirements. The addition of Section 7.7 of the code proposes a set of city-wide Pedestrian Connectivity Standards to guide the development of a walkway system that is continuous, safe, direct, and convenient and includes requirements for vehicle/walkway separation and walkway width and surface. In addition, code amendments in Section 6.7(d) require the establishment of maximum setbacks in the downtown area to ensure that the ground floors of buildings engage the street right-of-way, consistent with Objective 5.

2. APPLICABLE OREGON STATEWIDE PLANNING GOALS

2.1. Goal 1: Citizen Involvement

Goal 1 calls for "the opportunity for citizens to be involved in all phases of the planning process." It requires each city and county to have a citizen involvement program containing six components specified in the goal. It also requires local governments to have a committee for citizen involvement (CCI) to monitor and encourage public participation in planning.

Findings:

During the 1987 update of the City's Comprehensive Plan, a Committee for Citizen Involvement was formed and given the responsibility of ensuring citizen involvement through the use of media, community surveys, informational materials, public work sessions, and hearings. A Citizen's Advisory Committee was later created to translate the community's desires and needs into policy statements and guide the development of a land use plan. It has played a role in subsequent plan updates as well. In 2014, the City of Mt. Angel was the subject of a University of Oregon "Green Cities" class, which included holding a Design Charrette Community Workshop (See Exhibit B) to gather community input. In addition, interviews were conducted with stakeholders throughout the community during the process of the code assistance project (See Exhibit C).

2.2. Goal 2: Land Use Planning

Goal 2 outlines the basic procedures of Oregon's statewide planning program. It says that land use decisions are to be made in accordance with a comprehensive plan, and that suitable implementation ordinances to put the plan's policies into effect must be adopted. It requires that plans be based on "factual information"; that local plans and ordinances be coordinated with those of other jurisdictions and agencies; and that plans be reviewed periodically and amended as needed. Goal 2 also contains standards for taking exceptions to statewide goals. An exception may be taken when a statewide goal cannot or should not be applied to a particular area or situation.

Findings:

The proposed amendments revise and clarify language in the development code to improve consistency with the comprehensive plan and align more closely with the City's future development goals.

2.3 Goal 9: Economy of the State

Goal 9 states the need to provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens.

Findings:

The proposed amendments clarify language in the Development Code, making development permit procedures in commercial, as well as residential, areas more predictable. The revisions help to improve the overall function of the code, for applicants and staff, improving its reliability and user-friendliness. These amendments serve to strengthen the downtown core and its Bavarian theme, with the intention of making Mt. Angel a more desirable place for shoppers and a more profitable place in which to do business.

2.3. Goal 10: Housing

This goal specifies that each city must plan for and accommodate needed housing types, such as multifamily and manufactured housing. It requires each city to inventory its buildable residential lands, project future needs for such lands, and plan and zone enough buildable land to meet those needs. It also prohibits local plans from discriminating against needed housing types.

Findings:

The proposed amendments provide clear and objective design standards for residential uses to ensure quality housing at reasonable costs, without placing undue burden on the developer. The amendments are intended to simplify the review process for single-family developments by establishing clear and objective standards that add certainty to the development review process for applicants and staff.

2.4. Goal 12: Transportation

The goal aims to provide "a safe, convenient and economic transportation system." It asks for communities to address the needs of the "transportation disadvantaged."

Findings:

The proposed amendments bolster the downtown pedestrian environment and help the city to improve the multi-modal nature of downtown by providing safe and convenient access for walking and biking.

RECOMMENDATION:

The proposed updates to the Mt. Angel Development Code would help the city enhance its downtown area, strengthen regulations that control downtown development, and improve efficiency in the land use process. Staff finds the proposal consistent with pertinent Statewide Planning Goals and the goals and policies outlined in the Mt. Angel Comprehensive Plan.

Based on the findings identified in this report, staff recommends **APPROVAL** of the proposed amendments to the Mt. Angel Development Code.

PLANNING COMMISSION ACTION:

- A. I move that the Planning Commission approve the proposed amendments to the Mt. Angel Development Code, based upon the findings as presented in the staff report; or
- B. I move that the Planning Commission approve the proposed amendments to the Mt. Angel Development Code with modifications as determined by the Planning Commission (stating the modifications as part of the motion); or
- C. I move that the Planning Commission deny the proposed amendments to the Mt. Angel Development Code (stating the reasons/findings as to why the amendments should be denied); or
- D. I move to continue the request for the proposed amendments to the Mt. Angel Development Code to a time certain or indefinitely (stating at what point the Planning Commission would consider reopening the hearing.)

Note: The City Council adopts an ordinance through procedures by the City of Mt. Angel to grant final approval of the amendments to the Mt. Angel Development Code.

Memorandum

Date: November 28, 2016
To: Justin Hogue, City of Mt. Angel
cc: Gary Fish and Angela Carnahan, DLCD
 Naomi Zwerdling, ODOT
From: Cathy Corliss, AICP, Principal
Re: Mt Angel Code Assistance - Task 8.3 Draft Text Amendments

Overview

The goal of this TGM¹ Code Assistance project is to create recommendations for updating Mt. Angel's Development Code in order to help the city enhance its downtown area, strengthen regulations that control downtown development, and improve efficiency in the land use process. Specifically, identified objectives for this project are:

- Emphasize the pedestrian environment, particularly in the downtown core, to strengthen the Bavarian theme and ensure future projects will be consistent with, and enhance, downtown development.
- Help the city improve the multi-modal nature of the downtown area by providing safe and convenient access for walking and biking.
- Assess the city's Development Code to identify areas where revisions may be needed to improve the overall function, reliability and user-friendliness of the code. *Note: this objective was identified as a priority at the City Council-Planning Commission work session on July 16, 2015.*

This document includes proposed draft amendments to the City of Mount Angel Development Code. Additions to the code are shown in double-underline and deletions are shown in strike-through. For brevity, some sections of the Development Code not proposed for amendment are not included. These gaps are indicated by three asterisks "***". These draft code amendments are based on the recommendations from Phase 1 (Task 4.2 Final Action Plan and Report **(FAP)**, including Appendix A (Code Clean-Up Suggestions)) as well as input from the Project Management Team and suggestions from the Planning Commission and City Council at the joint work sessions on September 6th and 27th.

¹*This project is partially funded by a grant from the Transportation and Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Moving Ahead for Progress in the 21st Century (MAP-21), local government, and the State of Oregon funds. The contents of this document do not necessarily reflect views or policies of the State of Oregon.*

**SECTION 1
INTRODUCTORY PROVISIONS**

- 1.1 GENERAL PROVISIONS**
- 1.2 TITLE**
- 1.3 PURPOSE**
- 1.4 CONFORMANCE REQUIRED**
- 1.5 ORDINANCE ADMINISTRATION**
- 1.6 VIOLATIONS**
- 1.7 INTERPRETATION**
- 1.8 SIMILAR USE DETERMINATION**
- 1.9 SAVINGS CLAUSE**
- 1.10 DEFINITIONS**
- 1.11 GENERAL PROVISIONS**

1.10 DEFINITIONS

(b) Definitions: The following words and phrases, when used in this Ordinance, shall have the meanings set forth in this Chapter, except in those instances where the context clearly indicates a different meaning.

Accessory Structure: A detached, subordinate building, ~~or portion of a main building,~~ the use of which is incidental to that of the main building or to the use of the land, but does not include dwellings or living quarters.

~~Clear Vision Area: A triangular area on a lot or parcel at the intersection of two (2) streets or a street and a railroad, two (2) sides of which are lines measured from the corner intersection of the right of way lines. The third side of the triangle is a line across the corner of the lot or parcel joining the ends of the other two sides. Where the lines at the intersections have rounded corners, the right of way lines will be extended in a straight line to a point of intersection.~~

Dwelling, Attached Single Family (townhouse): A dwelling unit designed exclusively for occupancy by one (1) family located on its own lot that shares one or more common or abutting walls with one or more dwelling units on adjacent lot(s).

Façade: All the wall planes of a structure as seen from one side or view. For example, the front facade of a building would include all of the wall area that would be shown on the front elevation of the building plans.

Vision Clearance Area (also "Clear-Vision Area"): Formed by measuring from the intersection of the street corner or street and/or railroad corner property lines to points 30 feet along the front property line and side property line and then connecting the two points of base with a straight line.

**SECTION 2
ADMINISTRATIVE PROVISIONS**

- 2.1 SUMMARY OF APPLICATION TYPES AND REVIEW PROCEDURES**
- 2.2 GENERAL PROVISIONS**
- 2.3 NOTIFICATION OF HEARING**
- 2.4 HEARING PROCEDURES**
- 2.5 RECORD OF HEARING**
- 2.6 LIMITS ON ORAL TESTIMONY**
- 2.7 EXHIBITS**
- 2.8 CONTINUED HEARINGS**
- 2.9 APPEAL OF CITY ~~MANAGER~~ ADMINISTRATOR DECISIONS**
- 2.10 APPEAL OF PLANNING COMMISSION ACTIONS**
- 2.11 CITY COUNCIL ACTION**
- 2.12 SUPPLEMENTAL APPLICATION FOR REMAINING PERMITTED USES FOLLOWING DENIAL OF INITIAL APPLICATION**
- 2.13 FEES**

2.2 GENERAL PROVISIONS

(e) Quasi-Judicial Actions. This subsection establishes the procedures to be followed in Type II and Type III land use actions.

(1) Initiation: An application may be submitted by the property owner, contract purchaser or an authorized agent of the owner or contract purchaser. A pre-application conference is required prior to submittal of an application.

(2) Completeness: Upon receipt of an application, the City staff shall review the application for completeness with respect to the submission requirements of this Ordinance. If the application is incomplete, Staff shall notify the applicant of exactly what information is missing within ~~ten (10) days~~ 30 days of the receipt of the application, and allow the applicant to submit the missing information. The application shall not be acted upon until:

(A) All necessary information is received and the application is deemed complete; or

(B) Should the applicant refuse or fail to submit the missing information, the application will be deemed complete, for the purpose of acting on the application, ~~on the 31st day~~ 180 days after the original submission. Incompleteness of an application may be grounds for denial.

(3) Information submitted to the City after the date the application is deemed complete that results in a substantial change from the original application shall authorize review as a new application. The City Planner shall determine whether a submission constitutes a substantial change from the original application.

(4) Notice Procedures. ~~See Section 2.3 Notification of Hearing. Notice of a public hearing on a Type II or Type III, quasi-judicial application shall be given by the City by posting such notice in a conspicuous place on the subject property. The mailing of copy of the notice to property owners in the affected area shall be within 20 days prior to the date of the hearing. The affected area is herein defined as the land area that is within a 100-foot radius of the subject property. The failure of any such property owner to receive such notice by mail shall not affect the validity of the proceedings. The City staff shall file with the Planning Commission a certificate showing the date of mailing.~~

(8) Notice of Decision: Within ~~14~~12 days of any action on a quasi-judicial land use application, the applicant, ~~adjacent property owners within 100 feet of the subject property, and any group or participant who submitted oral or written public testimony for the hearing or requested notice of the decision, all individuals who have in writing requested notice of the decision,~~ shall be mailed written notice of the action. Notice shall specify findings justifying the approval or denial of the request and any applicable conditions of approval.

(g) Pre-Application Conferences.

(1) Purpose: The purpose of the pre-application conference is to familiarize the applicant with applicable requirements of the City's Development Code.

(2) Applicability: Pre-application conferences are required for all Type II or Type III development actions. This requirement may be waived by the City Planner.

(3) The pre-application may end up not covering all potential issues, and does not limit or hinder the City from enforcing regulations applicable to the development action.

(4) A written summary of the pre-application conference will be provided by the City Planner within approximately 14 days of the conference.

(5) If a complete application relating to a proposed development action that was the subject of a pre-application conference has not been submitted within eighteen (18) months of the conference, the applicant shall schedule a follow-up pre-application conference.

2.3 NOTIFICATION OF HEARING**(a) Quasi-Judicial Hearings (Type II and Type III).**

~~(1)~~ Notice of any public hearings before the Planning Commission or City Council shall be provided for any quasi-judicial land use action.

~~(1) Notice shall be published in a newspaper of general circulation a minimum of 20 days prior to the public hearing.~~ (2) Written notice of the initial public hearing shall be mailed at least 20 days prior to the hearing date to:

(A) The owners of record of real property within 100 feet of the boundaries of the subject property. The failure of any such property owner to receive such notice by mail shall not affect the validity of the proceedings;

(B) Any person who submits a written request to receive a notice; and

(C) Any governmental agency that is entitled to notice under an intergovernmental agreement entered into with the City and any other affected agencies. The failure of another agency to respond with written comments on a pending application shall not invalidate an action or permit approval made by the City under this Code.

(2) At least 14 days before the first hearing, the applicant or applicant's representative shall post notice of the hearing on the project site in clear view from a public right-of-way using a poster format prescribed by the City Planning Official.

(3) At least 14 days before the first hearing, the City shall publish notice of the hearing on the City website, and/or have said notice published in a newspaper with local circulation.

(b) Legislative Hearings (Type IV). Notice of Public Hearing by the Planning Commission or City Council relating to any legislative action shall be published in a newspaper of general circulation a minimum of 10 days prior to the date of the hearing. Notice shall be provided to the Department of Land Conservation and Development at least ~~35~~ 45 days prior to the first evidentiary hearing by the City for any legislative action.

SECTION 5 CONDITIONAL USES

5.1 PURPOSE

5.2 PROCESS

5.3 SUBMITTAL REQUIREMENTS

5.4 CONDITIONAL USE AND CONCURRENT VARIANCES

5.5 CRITERIA FOR APPROVAL

5.6 CONDITIONS

5.7 EFFECTIVE DATE OF CONDITIONAL USE

5.8 PERIOD OF VALIDITY

5.9 VIOLATION OF THE CONDITIONAL USE

5.10 AUTOMATIC CONDITIONAL USE STATUS

5.10 Automatic Conditional Use Status

Over time, the zoning regulations applicable to a specific site may change. This may be a result of changes to the content of the zoning regulations for a specific zone or from a change to the zoning map, including annexation rezonings. After one of these changes, if an existing use was allowed by right or was a nonconforming use, and is now listed as a conditional use, the use is considered an approved conditional use and may continue to operate. Any changes to the use are subject to the requirements and approval criteria of this section.

**SECTION 6
ZONING**

- 6.1. RESIDENTIAL ZONE, SINGLE FAMILY (RS)**
- 6.2. RESIDENTIAL ZONE, MULTI-FAMILY (RM)**
- 6.3. RESIDENTIAL COMMERCIAL ZONE (RC)**
- 6.4. GENERAL COMMERCIAL ZONE (CG)**
- 6.5. LIGHT INDUSTRIAL ZONE (IL)**
- 6.6. PUBLIC AND SEMI-PUBLIC USE ZONES (PS)**
- 6.7. DOWNTOWN CORE OVERLAY**

6.1 RESIDENTIAL ZONE, SINGLE FAMILY (RS)

(a) Purpose: It is the purpose of the RS Zone to permit single-family residential uses and their accessory structures and to permit, with Planning Commission approval, certain other uses that are compatible to single-family residential living.

(b) Permitted Uses. The following uses are permitted in the RS Zone:

- (1) Single-family dwelling, including a single-family manufactured home;
- (2) Public parks and recreation area;
- (3) Public buildings and structures, such as libraries and fire stations, with a maximum height of 35 feet or less;
- (4) Gardens, orchards and crop cultivation, in addition to those uses allowed in Ordinance, provided no stables or barns, livestock, bees or poultry are maintained in connection therewith, and provided further, that all other applicable ordinances are complied with;
- (5) Utility right-of-ways;
- (6) Public utility facilities of non-industrial nature;
- (7) Accessory uses and structures;
 - (A) Customary residential accessory buildings for private use, such as a pergola, greenhouse, hot house, hobby shop, or hobby house, summer house, patio, enclosed or covered patio, woodshed, quarters for domestic animals maintained as pets of the residents;
 - (B) Fences;
 - (C) Off-street storage for a commercial vehicle with a maximum of one commercial vehicle per dwelling;
 - (D) Guest houses and guest quarters not in the main buildings provided such houses and quarters are and remain dependent upon the main building for either or both the kitchen and bathroom facilities and the guest facilities are not used for residential purposes.
 - (E) Swimming pools for private use (requires a building permit);
 - (F) Home occupations, as defined in Section 1.10;
 - (G) Amateur radio antennas;
 - (H) Satellite dishes not exceeding one (1) meter in diameter;

(8) Right-of-way for streets

(c) Transitional Uses. The following transitional uses shall be permitted in the RS Zone where the side of a lot or parcel abuts upon any Commercial Zone or Industrial District, provided that such transitional use does not extend across a street or alley and in no case more than 165 feet from the boundary of the less restricted zone which it adjoins:

- (1) Two-family dwelling (duplexes) on a lot of 8,000 square feet or more;
- (2) The following public and semi-public use buildings and structures, with a maximum height of 35 feet or less:
 - ~~(A)(3)~~ Churches;
 - ~~(B)(4)~~ Community or neighborhood centers, including swimming pools and other allied facilities when erected by a nonprofit community organization for the improvement of the zone or for social recreation of the organization's members;
 - ~~(C)(5)~~ Public automobile parking areas when located and developed as prescribed in Section 8;
- ~~(3)(6)~~ Outdoor nursery for growing, displaying and sale of shrubs or plants.
- ~~(4)(7)~~ Day nursery, provided the residential character of the building is unchanged.

(d) Conditional Uses. The following uses are permitted as conditional uses, provided that such uses are approved in accordance with Section 5.

- (1) Public and semi-public use buildings and structures except as permitted by Subsections (b) and (c), above:
- (2) Radio, television, and other telecommunications transmitters and antennae;
- (3) Public or private schools;
- (4) Two-family dwelling (duplexes) on a lot of 8,000 square feet or more;
- (5) Day nursery, provided the residential character of the building is unchanged;
- (6) Use of an accessory building for a home occupation;
- (7) Planned Unit Development subject to the provisions of Section 10.5;
- (8) Manufactured Home Parks subject to the provisions of Section 10.4; 10.5;
- (9) Golf Course;
- (10) Conditional Home Occupations subject to the provisions of Section 10.2; 10.5;
- (11) Boat, camper, and trailer storage area on lot or parcel incidental to the permitted use subject to the provisions of Section 10.3;
- (12) Bed and Breakfast subject to the provisions of Section 10.6;
- (13) Zero side yard dwellings units subject to the provisions of Section 10.7; 1.
- (14) Satellite dishes greater than one (1) meter in diameter.

(e) Dimensional Standards

The following dimensional standards shall be the minimum requirements for all development in the RS Zone.

(1) Minimum Lot Area

(A) Interior Lot 7,000 square feet

(B) Corner Lot 8,000 square feet

(C) Within the Infill Development Overlay Zone smaller lot sizes may be approved subject to the standard in Section 14. ~~Vacant parcels with a minimum size of 5,600 square feet may be created, subject to the design requirements of Section 14.6 Residential Development Standards.~~ No variances to the minimum lot size or design requirements will be allowed.

(2) Minimum Yard Setback Requirements. All principal ~~and accessory~~ structures shall maintain the following minimum yard setbacks. Projections into setbacks are subject to Section 7.1. Accessory structures shall be subject to the setback requirements in Section 7.2.:

(A) Front Yard 20 feet

(B) Rear Yard Setbacks

- (i) One-story building 24 feet
- (ii) Two-story building 30 feet
- (iii) 2-and-one-half story building 36 feet
- (iv) Corner Lot 14 feet

(C) Side Yard Setbacks

- (i) One-story building 5 feet
- (ii) Two+ -story building 6 feet

(4) Minimum Lot Width

- (A) Front Lot Line Width 20 feet
- (B) Interior Lot Width at Building Line 50 feet
- ~~(B) Corner Lot Width at Building Line 70 feet~~

(f) Single Family Dwelling Features. New single-family homes and duplexes shall comply with the design standards in Section 7.4. ~~All single-family dwellings shall have a private garage constructed of like materials and design to include 12-inch projecting eaves on all Mt. Angel Development Regulations Page 6-5 structures. All single-family dwelling shall utilize at least three (3) of the following design features to provide visual relief along the front of the home:~~

~~(1) Dormers;~~

- ~~(2) Gables;~~
- ~~(3) Recessed entries;~~
- ~~(4) Covered porch entries;~~
- ~~(5) Cupolas;~~
- ~~(6) Pillars or posts;~~
- ~~(7) Bay or bow windows;~~
- ~~(8) Offsets on building face or roof (minimum 16")~~

6.2 RESIDENTIAL ZONE, MULTI-FAMILY (RM)

(b) Permitted Uses. The following uses are permitted in the RM Zone:

- (1) Any permitted, transitional, and conditional use allowed in the RS Zone except for uses listed as Conditional Uses in Subsection (c) below: Bed and Breakfasts;
- (2) Duplexes, Attached Single Family Dwellings (Townhouses), Multi-Family Dwellings, and Apartments;

(d) Dimensional Standards

The following dimensional standards shall be the minimum requirements for all developments in the RM Zone.

(1) Minimum Lot Area

(A) Single-family dwelling

- (i) Interior Lot 7,000 square feet
- (ii) Corner Lot 8,000 square feet

(iii) Lot sizes may be reduced by up to 20% subject to the Incentive Land Division standards in Section 7.4(d).

(B) Duplex: 8,000 square feet

(C) Multi-family dwelling, three (3) unit 10,500 square feet
 plus 3,500 square feet per unit in excess of 3 units

(D) Attached Single Family Dwelling (Townhouse) 3,500 sf for each attached single family dwelling

(2) Minimum Yard Setback Requirements. All principal ~~and accessory~~ structures shall maintain the following minimum yard setbacks. Projections into setbacks are subject to Section 7.1. Accessory structures shall be subject to the setback requirements in Section 7.2.;

(A) Front Yard 15 feet

No parking shall be allowed in the required minimum front yard exclusive of the driveways.

(B) Rear Yard

- (i) One-story building 10 feet
- (ii) Two-story building 15 feet
- (iii) Over Two stories 20 feet

No parking shall be allowed within ten (10) feet of the street property line.

(C) Side Yard. The width of the side yard on each side of the lot shall not be less than one-half the height of the building, provided that:

- (i) No side yard shall be less than five (5) feet nor required to be more than 15 feet.
- (ii) The side yard of a street shall not be less than 15 feet and may not be utilized for parking within ten (10) feet of the street property line.
- (iii) The setback for the common wall of an attached single family dwelling (Townhouse) can be reduced to zero (0) in accordance with Section 7.5.

(5) Minimum Lot Width

- (A) Width at Front Lot Line 20 feet
- (B) Interior Lot Width at Building Line 50 feet
- ~~(C) Corner Lot Width at Building Line 70 feet~~
- (C) Interior Lot Width at Building Line (Townhouse) 24 feet

6.3 RESIDENTIAL COMMERCIAL ZONE (RC)

(b) Permitted Uses. The following uses are permitted in the RC Zone:
 Any permitted, transitional and conditional uses allowed in the RM Zone with the exception of uses listed as Conditional Uses in Subsection (c) below; ~~a Bed and Breakfast;~~

(d) Dimensional Standards. The following dimensional standards shall be the minimum requirements for all developments in the RC Zone.

(1) Minimum Lot ~~Area~~ Coverage

(A) Residential lot coverage is the same as in the RM Zone.

(B) All other uses shall occupy no more than 60 percent of the lot area.

(2) Minimum Yard Setback Requirements. Residential developments and residential accessory structures shall be subject to the setback requirements of the RM zone. All other principal and accessory structures shall maintain the following minimum yard setbacks:

- | | |
|-------------------------------------------------|--------------------|
| (A) Front Yard | 15 feet |
| (B) Side Yard | |
| (i) Adjoining a non-residential zone | None |
| (ii) Adjoining a residential zone | 20 feet |
| (C) Rear Yard | |
| (i) One-story dwelling building | 10 feet |
| (ii) Two-story dwelling building | 15 feet |
| (iii) Two+ storied dwelling building | 20 feet |
| (iv) One story building | 10 feet |
| (v) Two story building | 15 feet |

(3) Landscaped Yards. For residential purposes, all required yards adjacent to a street shall be landscaped including along the perimeter of parking areas. Non-residential land uses shall provide landscaping as required below:

- (A) Provide for combined perimeter and interior landscaping along parking areas and driveways.
- (B) Provide minimum five-foot high solid screen when adjacent to land uses other than commercial or industrial.
- (C) Provide a visual relief screen when adjacent to any street in the form of a hedge, fence, planter box, berm, shrubbery, and trees or any combination thereof.
- (D) A vision clearance area shall be observed regarding all screens.
- (E) Landscaping shall be provided on at least 15 percent of the lot area.

(4) Lot Width. Lot width is the same as in the RM Zone.

(5) Maximum structure height. Maximum structure height is the same as in the RM Zone.

6.4 GENERAL COMMERCIAL ZONE (CG)

(b) Permitted Uses. The following uses are permitted in the General Commercial Zone:

(11) The following businesses shall be permitted, provided that:

- The lot is paved with a concrete or asphalt surface;
- The lot is screened from any adjoining residential zone by a sight-obscuring fence, wall, or hedge at least five (5) feet in height; and
- All storage of merchandise and supplies shall be conducted wholly within a building or enclosed area, except for the provision of gas services ancillary to a repair or service station provided below.

(A) Automobile, truck, motorcycle, trailer, farm equipment, recreational vehicle and boat sales and repair;

(B) Automobile service station, including towing services and vehicle washing and polishing facilities, and services;

(C) Part and accessory sales for automobiles, trucks, motorcycles, trailers, farm equipment, recreational vehicles and boats;

(D) Retail tire sales, service and repair; tire recapping (conducted wholly within a building), service and repair, paint and body shop;

(E) Lumber yard and contracting supplies for lumber, stone, masonry or metal (sales only);

(F) Special trade contracting facilities such as floor laying, building equipment, masonry and stone, plumbing, electrical, metal work or painting;

(G) Newspaper, periodical, publishing and printing;

(H) Veterinary clinics;

(I) ~~Tire recapping (conducted wholly within a building); welding; blacksmith shop; public swimming pool; travel trailer park; contracting supplies, lumber, stone, masonry, metal (sales only); garage and repair shop; newspaper, periodical, publishing and printing; tractor and farm equipment, sales, and service; veterinary clinic; kennel; light components assembly; baker (wholesale and manufacture of bakery products)~~

(J) Cabinet shop (conducted wholly within a building)

(12) Accessory uses and structures;

(A) Amateur radio antennas;

(B) Satellite dishes not exceeding two (2) meters in diameter;

(C) Collocated wireless communication facilities where the existing support structure height will not be increased by 20 feet or more.

(c) Conditional Uses. The following uses are will be permitted as conditional uses, provided that such uses are approved in accordance with Section 5:

- (1) Self-service, short-term storage;
- (2) Public swimming pool except as permitted above;
- (3) Recreational vehicle park except as permitted above;
- (4) Kennel except as permitted above;
- (5) Light components assembly except as permitted above;
- (6) Bakery: wholesale and manufacture of bakery products except as permitted above;
- (7) Mini-storage warehouses.
- (8) Satellite dishes greater than two (2) meters in diameter;
- (9) Towers and ancillary wireless communication facilities, subject to the provisions of Section 18, provided that the facilities are not located within 350 feet of any residential zones.
- (10) Collocated wireless communication facilities where the existing support structure height will be increased by 20 feet or more.
- (11) Medical Marijuana Facilities subject to the provisions of Section 10.8.

(d) Dimensional Standards

The following minimum dimensional standards shall be required for all development in the General Commercial Zone. Projections into setbacks are subject to Section 7.1. Accessory structures used in connection with residential purposes shall be subject to the setback requirements in Section 7.2.

6.5 LIGHT INDUSTRIAL ZONE (IL)

(b) Permitted Uses. The following uses are permitted in the Light Industrial Zone:

(1) Any permitted or conditional use allowed in the CG Zone except ~~Medical Marijuana Facilities~~ for uses listed as Conditional Uses in Subsection (c) below;

(c) Conditional Uses.

(12) Collocated wireless communication facilities where the existing support structure height will ~~not~~ be increased by 20 feet or more.

6.7 DOWNTOWN CORE ZONE OVERLAY DISTRICT (DC)

(a) Purpose: To provide for an appropriate range of mixed use retail, service, and residential uses that are primarily dependent on pedestrian patronage. Except as modified below, the standards of the underlying zone apply.

(b) Permitted Uses. The following uses are permitted within the base zones in the Downtown Core Overlay District: ~~Commercial Zone:~~

~~(1) All uses described in the General Commercial Zone except those listed in Section 6.4 (b) (11).~~

<u>Base Zone</u>	<u>Permitted Uses</u>	<u>Conditional Uses</u>
RC	<u>All permitted uses in the RC zone and permitted use in the CG zone except those listed in Section 6.4 (b)(11)</u>	<u>All conditional uses in the RC zone</u>
CG	<u>Permitted uses in the CG zone except those listed in Section 6.4 (b)(11)</u>	<u>All conditional uses in the CG zone</u>
IL	<u>All permitted uses in the IL zone</u>	<u>All conditional uses in the IL zone except for the following:</u> <ul style="list-style-type: none"> • <u>Chemicals, fertilizers, insecticides, paint, and allied products manufacturing facilities;</u> • <u>Paper and allied products manufacturing facilities;</u> • <u>Petroleum products and gasoline storage, provided all storage is underground;</u> • <u>Transportation equipment manufacturing facilities, including but not limited to motor vehicle brakes and railroad equipment;</u> • <u>Wood and lumber products processing, manufacturing, and storage facilities;</u> • <u>Auto racing track; and</u> • <u>Mining, pits, and quarries facilities.</u>

(c) Dimensional Standards. The following dimensional standards shall be required for all development in the Downtown Core Zone. Projections into setbacks are subject to Section 7.1. Accessory structures used in connection with residential purposes shall be subject to the setback requirements in Section 7.2.

(1) Minimum Yard Setbacks

(A) Front Yard	none
(B) Side Yard	
Adjoining a non-residential zone	none
Adjoining a residential zone	15 feet
(C) Rear Yard	
Adjoining a non-residential zone	none
Adjoining a residential zone	15 feet
(D) Maximum Building Height	45 feet

(d) Additional Development and Design Standards. The intent of these standards is to enhance the pedestrian environment in downtown Mt. Angel, provide civic gathering spaces for employees and visitors, support natural surveillance of public spaces, and encourage comfortable, human-scale design.

(1) Applicability. These standards apply to all new buildings and building expansions of more than 25% of the gross floor area.

(2) Maximum setback. Required maximum setbacks are established to ensure that the ground floors of buildings engage the street right-of-way. The maximum setback ensures compatibility and harmony between buildings, enabling a series of different buildings to maintain or establish a continuous vertical street wall. Buildings subject to this section shall conform to the following standards:

(A) The maximum setback is ten (10) feet. The standard is met when at least 50 percent of the street frontage has the front façade of a building placed within ten (10) feet of the street facing property line. On corner lots and through lots this standard must only be met on one street frontage, provided that on corner lots no parking or vehicular circulation shall be located between the building and the street corner.

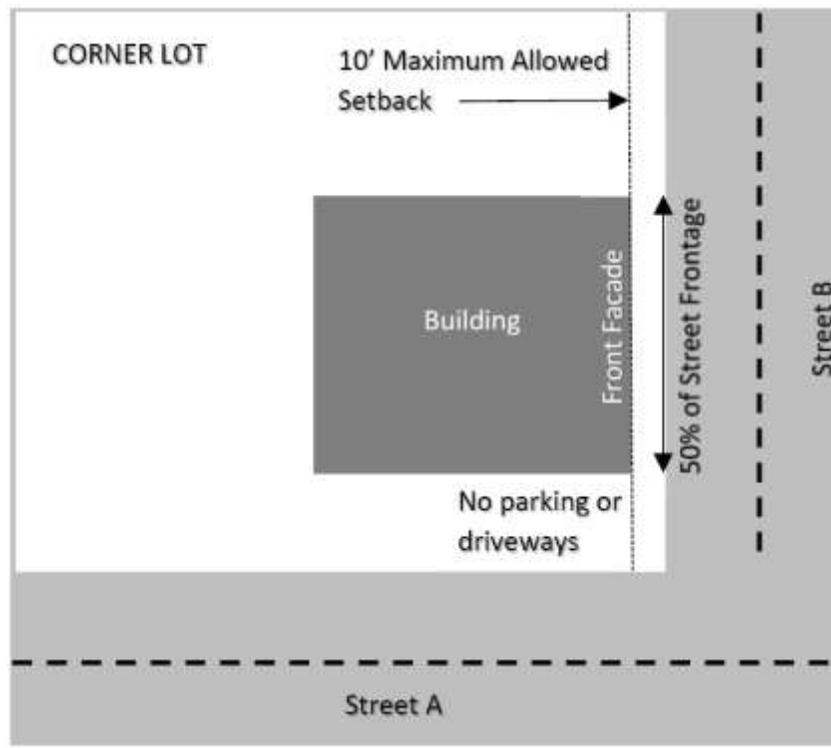
(B) The City, through Site Design Review, may waive or modify the maximum setback standard where it finds that one or more of the conditions in subsections (i)-(vii) occurs.

(i) A proposed building is adjacent to a single-family dwelling, and an increased setback promotes compatibility with the adjacent dwelling.

(ii) The standards of the roadway authority preclude development in conformance with the maximum setback.

- (iii) The applicant proposes extending an adjacent sidewalk or plaza for public use, or some other pedestrian amenity is proposed to be placed between the building and public right-of-way.
- (iv) The maximum setback may be increased to provide a private open space (e.g., landscaped forecourt) between a use in a mixed-use development (e.g., live-work building with ground floor residence) and a front or street property line.
- (v) A significant tree or other environmental feature precludes strict adherence to the standard and will be retained and incorporated in the design of the project.
- (vi) A public utility easement or similar restricting legal condition that is outside the applicant's control makes conformance with the maximum setback impracticable. In this case, the building shall instead be placed as close to the street as possible given the legal constraint, and pedestrian amenities (e.g., plaza, courtyard, landscaping, outdoor seating area, etc.) shall be provided within the street setback in said location.
- (vii) An expansion is proposed on an existing building that was lawfully created but does not conform to the above standard, and the building addition moves in the direction of compliance where practicable.

Figure 6.7-1: Maximum Setback



(3) Primary Entrances. This standard promotes pedestrian-friendly development by providing building entrances that are oriented to the sidewalk or other public space and connected with clearly-marked pedestrian walkways.

(A) All new buildings shall have at least one primary entrance facing an abutting street (i.e., within 45° of the street property line) or, if the building entrance must be turned more than 45° from the street (i.e., front door is on a side or rear elevation) due to the configuration of the site or similar constraints, a pedestrian walkway must connect the primary entrance to the sidewalk.

(B) Where a development contains multiple buildings and there is insufficient street frontage to meet the above building orientation standards for all buildings on the subject site, a building's primary entrance may orient to plaza, courtyard, or similar pedestrian space containing pedestrian amenities. When oriented this way, the primary entrance(s), plaza, or courtyard shall be connected to the street by a pedestrian walkway.

(4) Windows and Doors. The standards of this section are intended to enhance street security and provide a comfortable pedestrian environment by providing ground-level transparency between the interior of buildings and the sidewalk.

(A) For new buildings, unless the view of the building from the street is obstructed by another building, a minimum of 40 percent of the ground-floor street wall area must consist of openings (i.e., windows or glazed doors). The ground-floor street wall area is defined as the area up to the finished ceiling height of the space facing the street or 15 feet above finished grade, whichever is less.

(B) Clear glazing is required for ground-floor windows.

(C) The bottom edge of windows facing the street shall be constructed no more than 36 inches above grade (this shall not preclude additional windows being located above these windows).

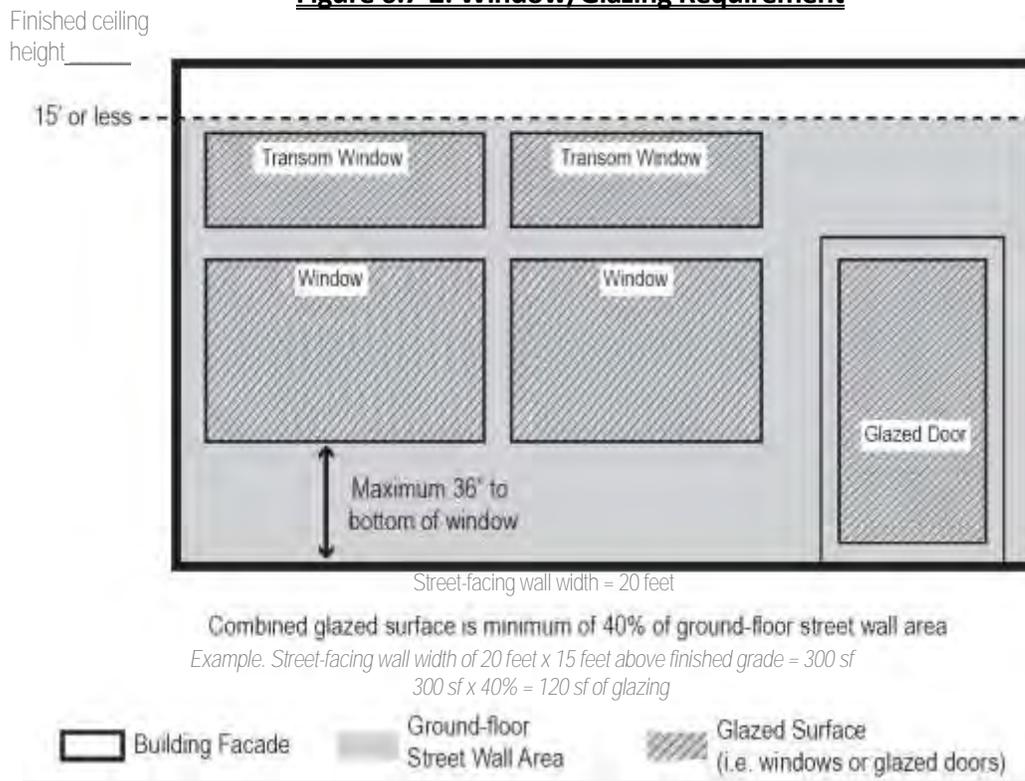
(D) Ground-floor windows for nonresidential uses shall allow views into storefronts, working areas, or lobbies. Signs are limited to a maximum coverage of 50 percent of the required window area.

(E) For all building windows facing streets, the following window elements are prohibited:

(i) Reflective, tinted, or opaque glazing.

(ii) Simulated divisions (internal or applied synthetic materials).

(iii) Exposed, unpainted metal frame windows.

Figure 6.7-2: Window/Glazing Requirement

(5) Rooftop Equipment and Screening. The intent of this standard is to integrate mechanical equipment into the overall building design.

(A) Equipment under two feet in height does not require screening.

(B) Elevator mechanical equipment may extend above the height limit a maximum of 16 feet provided that the mechanical shaft is incorporated into the architecture of the building.

(C) Satellite dishes, communications equipment and all other roof-mounted mechanical equipment shall be limited to 10 feet in height, shall be set back a minimum of five feet from the roof edge, and shall be screened from public view and from views from adjacent buildings by one of the following methods:

(i) A screen around the equipment that is made of a primary exterior finish material used on other portions of the building or wood fencing or masonry;

(ii) Green roof features or regularly maintained dense evergreen foliage that forms an opaque barrier when planted.

(D) Required screening shall not be included in the building's maximum height calculation.

**SECTION 7
GENERAL DEVELOPMENT STANDARDS**

7.1 YARD AND LOT STANDARDS

7.2 ACCESSORY STRUCTURES

7.3 HEIGHT AND OTHER EXCEPTIONS

7.4 SINGLE FAMILY DESIGN STANDARDS

7.5 MULTIFAMILY AND ATTACHED DWELLING DESIGN STANDARDS

7.6 PEDESTRIAN CONNECTIVITY STANDARDS

7.1 YARD AND LOT STANDARDS

(c) No Parking In Front Yard, Yards Adjacent To A Street, Or Landscaped Areas. No parking shall be allowed exclusive of driveways within the ~~required~~ front yard area. The side yard and rear yard areas may be used for parking vehicles unless otherwise prohibited by this ordinance.

The yard areas adjacent to a street shall not be used for the permanent storage of utility trailers, house or vacation trailers, boats, or other similar vehicles where such parking or storage might impair public safety and welfare.

(d) Average Yard Setback Adjacent To A Street (Front And Exterior Side Yards). Where a 20 foot minimum front setback is required, every ~~Every~~ building requiring a front yard shall set back from the front line at least 20 feet, except in the instance where the average depth of the other buildings on the same side of the street are between 30 and ten (10) feet, then the average depth may be used. The average depth is the average of the distance from the closest part of the foundation of the existing buildings to the front property line where the existing buildings are within 200 feet of the center of the proposed building, on the same side of the street, within the same block. If existing buildings are within less than ten (10) feet of the property line, then no less than ten feet shall be used in figuring the average, or if existing buildings are more than 30 feet from the property line then no more than 30 feet shall be used in figuring the average.

7.2 ACCESSORY STRUCTURES

(a) Application Of Regulations. The regulations herein set forth shall apply to all residential zones and to structures in any other zone used in connection with residential purposes. Accessory structures shall be located within the rear or interior yard. A maximum of two (2) are permitted.

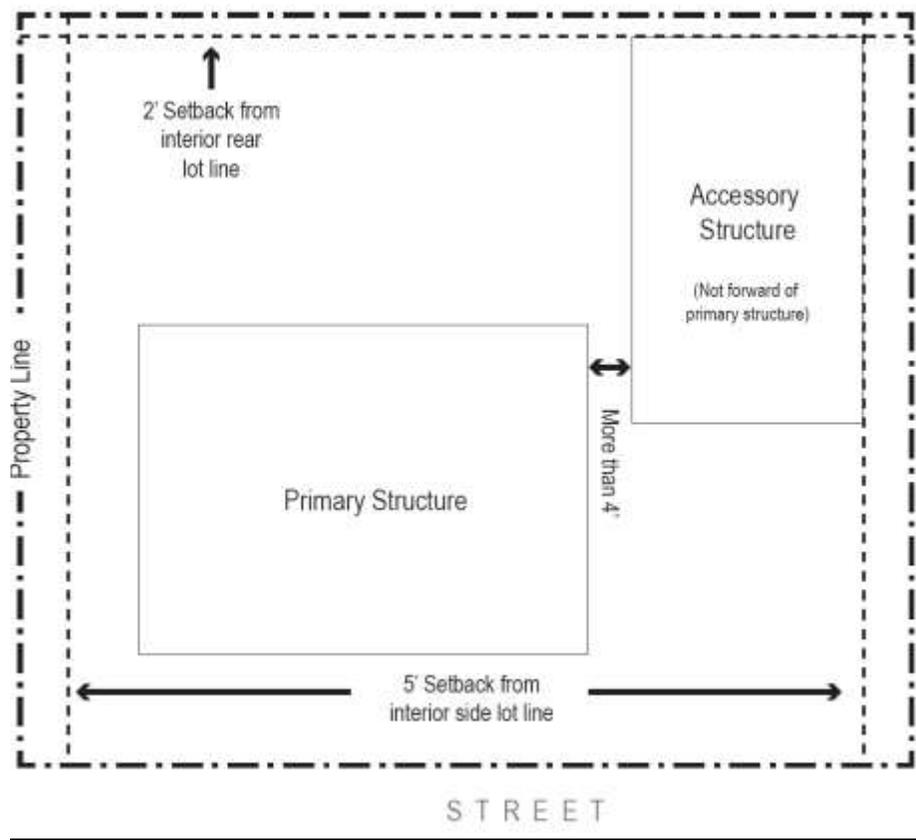
(b) Height. The maximum height of any accessory structure shall be eight (8) feet at the ~~lot line~~ minimum setback specified below. Such maximum height may be increased one (1) foot for each one (1) foot of distance from the minimum setback ~~lot line~~ to a maximum height of 20 feet. Roof drainage shall be accommodated within the confines of the property.

(c) Front Yards And Yards Adjacent To Streets. Any accessory structure, except fences, which has any portion extending above grade shall ~~observe the yard requirements the same as the main building~~ not be closer to the front lot line than the primary structure.

(d) Side Yards, Interior. Accessory structures not attached to the main building located in an interior side yard shall be set back at least five (5) feet from any lot line.

(e) Rear Yards. Within interior rear yards and portions of rear yards not abutting a street, accessory structures not attached to the main building shall be set back at least two (2) feet from any lot line ~~an accessory structure may be placed on the property line~~ except along an alley. All structures except fences shall be at least five (5) feet from the alley.

Figure 7.2-1: Accessory Structure Placement



(f) Accessory Structures Attached To The Main Building. Covered or enclosed accessory buildings which are attached to the main building shall be considered as a portion of the main building and shall observe the same requirements as the main building except for certain projections, as provided in Section 7.1. Accessory structures shall be considered attached to the main building when any portion of the accessory structure is located within 4 feet of the main building.

(g) Fences, Walls, And Hedges - Location, Height, And Density. Fences, walls, and hedges may be located in any required yard or along the edge of any yard, subject to the maintenance of required clear-vision areas. Fences along a front property line or within a front yard setback shall not exceed a height of five (5) feet, when the fence is at least 50 percent open. All other in fences, walls, or hedges along a front property line shall not exceed a height of four (4) feet. For side and rear yards, fences, walls or hedges shall not exceed six (6) feet in height without approval of a variance. Fences and walls associated with single family development are subject to the standards in Section 7.4.

7.4 SINGLE FAMILY DESIGN STANDARDS

(a) Purpose. The following standards are intended to ensure that single family development is compatible with existing buildings and neighborhoods and that the appearance of all neighborhoods is enhanced through creation of attractively designed housing.

(b) Applicability. These standards and requirements shall apply to all new single-family homes and each half of a duplex. Except as specifically provided in this Section, the standards and requirements of the underlying zoning and other Sections of this Ordinance shall apply.

(c) Design Standards.

(1) Review Process for Single-Family and Two-Family Dwellings. Single-family and two family dwellings shall meet the standards of this Section. Appeals of the City Manager's decision shall be to the Planning Commission.

(2) Except for flag lots, each single-family home and each half of a duplex shall have its front façade and front door oriented towards the street. Where public street frontage is not provided and lots are served by a private accessway, the fronts of residential buildings shall be oriented to the private accessway.

(3) All dwellings and accessory structures shall include eaves with a minimum projection of twelve (12) inches. All windows and doors on the primary street facing façade shall be provided with trim material (2 inch minimum nominal width). Dwellings on corner lots shall also provide windows and doors (with trim of 2 inch minimum nominal width) occupying a minimum of ten percent on all secondary street facing facades.

(4) All single-family dwellings shall have a private garage constructed of like materials and design. A garage may not be located closer to the front lot line than the rest of the dwelling. This may be measured from a covered front porch if applicable.

(5) New single-family and two family dwellings shall incorporate a minimum of five (5) design elements on the primary street facing façade. The list of design elements from which applicants shall choose is provided in Table 7.4-1. The minimum standard must be met in order for the design element to qualify as one of the required elements; however, applicants may choose to vary from these standards for non-required design elements.

Table 7.4-1: Design Elements

<u>Design Element</u>	<u>Minimum Standard</u>
<u>Dormers</u>	<u>Must be at least three (3) feet wide</u>
<u>Pitch Roof</u>	<u>Must be a hip and gable roof with a minimum pitch of 4/12 Jerkinhead and gambrel roofs can also be used.</u>
<u>Covered porch entry</u>	<u>Must be a minimum 40 square foot and a minimum five (5) feet deep</u>
<u>Front porch railing</u>	<u>Must be around at least two (2) sides of the porch</u>
<u>Front facing second story balcony</u>	<u>Must project from the wall of the building a minimum of four (4) feet and be enclosed by a railing or parapet wall</u>
<u>Building face off-sets</u>	<u>Must include two (2) or more off-sets of 16 inches or greater</u>
<u>Roof overhang</u>	<u>Must be 16 inches or greater</u>
<u>Columns, pillars or posts</u>	<u>Must be at least four (4) inches wide and contain larger base materials</u>
<u>Gables</u>	<u>Must be cross or diagonal bracing, shingles, trim, corbels, exposed rafter ends, or brackets</u>
<u>Siding of shakes, shingles, brick, natural stone, cultured stone, natural wood</u>	<u>Must occupy at least 60 square feet of the street façade</u>
<u>Differentiated base materials of natural stone, cultured stone, or brick</u>	<u>Must extend at least 36 inches above adjacent finished grade and occupy a minimum of 10 % of the overall primary street facing façade</u>
<u>Trim on windows and doors</u>	<u>Must be 4 inch minimum nominal width and 0.5 inch nominal depth on windows and doors on the primary street facing façade. Windows must have a window sill.</u>
<u>Vertically oriented windows</u>	<u>Must be oriented vertically, with proportions of at least 3:2 on all street facing windows. Large horizontally oriented "picture windows" are not permitted; a series of vertical windows may be used instead.</u>
<u>Recessed window glazing</u>	<u>Must be recessed minimum of 2 inches inside the window casing</u>
<u>Bay or bow windows</u>	<u>Must extend a minimum of 12 inches outward from the main wall of a building and forming a bay or alcove in a room within the building</u>
<u>Windows and front door</u>	<u>Must occupy a minimum of 15 percent of the primary street facing façade (not including the roof and excluding any windows in a garage door)</u>
<u>Sidelight and/or transom</u>	<u>Must be associated with the front door or windows in the front door</u>

<u>windows</u>	
<u>Window grids</u>	<u>Must be on all façade windows (excluding any windows in the garage door or front door)</u>
<u>Reduced garage door size</u>	<u>Must be either a maximum nine (9) foot wide garage door(s) or a garage door designed to resemble two smaller garage doors (i.e., 9 feet wide or less) (This element is only applicable to street facing garages).</u>
<u>Recessed front facing garage</u>	<u>Garage door is recessed at least two feet behind the primary street facing façade.(This element is only applicable to street facing garages)</u>
<u>Other items as proposed by the applicant meeting the intent of this section</u>	<u>Must be approved the City Manager or designee.</u>

(6) Fences and Walls.

(A) Fences and walls in front yards shall be no more than four (4) feet in height except that fences and walls located on side lot lines may be up to six (6) feet in height provided they are no closer than ten (10) feet from a street or a public access easement.

(B) Fences and walls in side and rear yards that are within ten (10) feet of a street or a public access easement shall be no more than four (4) feet in height.

(C) All other fences and walls may be up to six (6) feet in height.

(7) Development Pattern. Except as may be permitted through the planned unit development process, no more than four identical structures in a row shall be allowed.

7.5 ATTACHED SINGLE FAMILY DWELLING (TOWNHOUSE) DESIGN STANDARDS

(a) Purpose. The following standards are intended to ensure that attached single family development is compatible with existing building and neighborhoods and that the appearance of all neighborhoods is enhanced through creation of attractively designed housing.

(b) Applicability. These standards and requirements shall apply to all new attached single-family homes. Except as specifically provided in this section, the standards and requirements of the underlying zoning and other Sections of this Ordinance shall apply.

(c) Standards.

(1) Attached single family dwellings are permitted in those zones which permit multifamily dwellings on the ground floor.

(2) The minimum lot area: 3,500 sf for each attached single family dwelling.

(3) Interior Lot Width at Building Line: 24 feet.

(4) The setback requirements of the applicable zone shall be applied to the townhouse building(s) except that the setback for the common wall on a townhouse is reduced to zero (0).

(5) No more than four (4) single family attached dwellings may be attached in a single building.

(6) Each unit shall provide a minimum of seventy-two (72) square feet of contiguous private outside open area (patio/deck/lawn). The minimum width or depth of the open area(s) shall be four (4) feet.

(7) The single family design standards in Section 7.4 shall apply to attached single family dwellings. In addition, attached single family dwellings receiving access directly from a public or private street shall comply with all of the following standards. These standards are intended to minimize interruption of adjacent sidewalks by driveway entrances, slow traffic, improve appearance of the streets, and minimize paved surfaces for better storm water management. See the following Figure 7.5-1 - Townhomes with Street Access.

(A) When garages face the street, they shall either be:

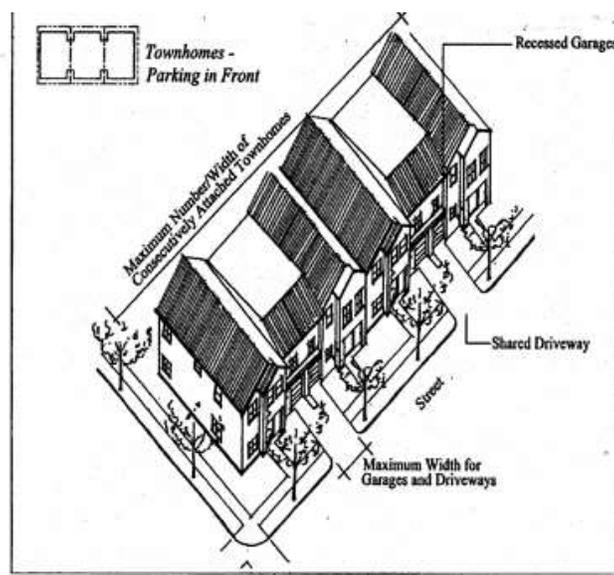
(i) Recessed behind the front elevation (i.e., living area or covered front porch) by a minimum of four (4) feet; or,

(ii) Flush with the front elevation, provided there is a balcony or living area above the garage that is either flush with the front elevation or projects beyond it.

(B) The maximum allowable driveway width facing the street is twelve (12) feet per dwelling unit.

(C) The maximum combined garage frontage per unit is fifty percent (50%) of the total building façade facing the street. For example, a unit with a twenty-four (24) foot wide façade facing the public street may have twelve (12)-feet of garage facing the street.

Figure 7.5-1 – Attached Single Family Dwellings with Street Access



7.6 MULTIFAMILY DESIGN STANDARDS

(a) Purpose. The following standards are intended to ensure that multifamily development is compatible with existing building and neighborhoods and that the appearance of all neighborhoods is enhanced through creation of attractively designed housing.

(b) Applicability. These standards and requirements shall apply to all new multifamily developments. Except as specifically provided in this Section, the standards and requirements of the underlying zone and other Sections of this Ordinance shall apply.

(c) Standards.

(1) Review Process for Multifamily. Multifamily buildings shall meet the standards of this Section.

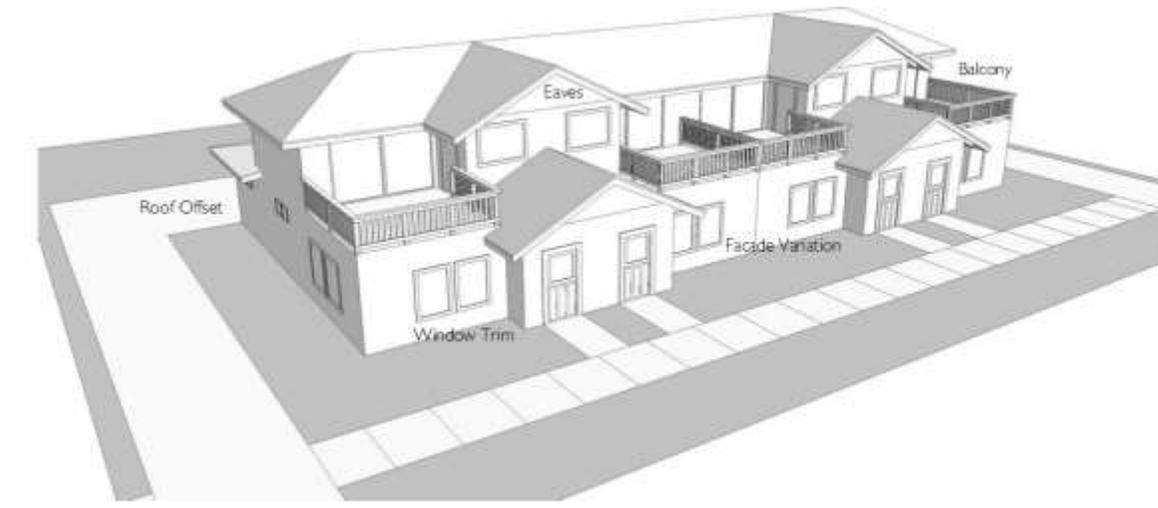
(2) Offsets and Variation. Plans for multifamily buildings shall incorporate design features such as varying rooflines, offsets, balconies, projections (e.g., overhangs, porches, or similar features), recessed or covered entrances, window reveals, or similar elements that break up otherwise long, uninterrupted elevations. Such elements shall occur at a minimum interval of 30 feet, and each floor shall contain at least two elements from the following options:

(A) Recess (e.g., porch, courtyard, entrance balcony, or similar feature) that has a minimum depth of four feet;

(B) Extension (e.g., floor area, porch, entrance, balcony, overhang, or similar feature) that projects a minimum of two feet and runs horizontally for a minimum length of four feet; or

(C) Offsets or breaks in roof elevation of two feet or greater in height.

Figure 7.6-1 – Multifamily Detailed Design



(3) Front Facades. All primary, ground-floor common entries or individual unit entries of street frontage units shall be oriented to the street, not to the building interior or to a parking lot.

(4) Main Entrance. Primary structures must be oriented with their main entrance facing the primary street frontage of the site. If the site is on a corner it may have its main entrance oriented to either street or to the corner.

(5) Parking. Parking and loading areas may not be located between the primary structure and right-of-way on which the structure fronts. If there is no alley and motor vehicle access is from the street, parking must be either in a garage that is attached to the primary structure, in a detached accessory structure located at least 50 feet from the front property line, or in a parking area at the side or rear of the site.

(6) Parking Lot Landscaping. Where more than four surface parking spaces are provided abutting a residential district or street, a minimum five (5)-foot-wide landscaped yard shall be established between the parking lot and abutting residential district or street. The landscaped yard area shall be planted with a continuous three (3)-foot-high hedge that will reach maturity within five (5) years.

(7) Screening. All mechanical, electrical, communications, and service equipment shall be screened from public view by parapets, walls, fences, landscaping, or other suitable means.

7.6 PEDESTRIAN CONNECTIVITY STANDARDS

(a) Purpose: The following standards are intended to provide for safe, reasonably direct and convenient pedestrian access and circulation. The standards also support and complement the recommended building design standards for the downtown core by emphasizing the pedestrian walkway system.

(b) Applicability: The following standards apply to all new commercial, public use and multi-family developments.

(c) Standards:

(1) Continuous Walkway System. A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.

(2) Safe, Direct, and Convenient. Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, recreational areas, playgrounds, and public rights-of-way conforming to the following standards:

(A) The walkway is reasonably direct. A walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-of-direction travel.

(B) The walkway is designed primarily for pedestrian safety and convenience, meaning it is reasonably free from hazards and provides a reasonably smooth and consistent surface and direct route of travel between destinations. The City may require landscape buffering between walkways and adjacent parking lots or driveways to mitigate safety concerns.

(C) The walkway network connects to all primary building entrances, consistent with the building design standards of this Code and, where required, Americans with Disabilities Act (ADA) requirements.

(3) Vehicle/Walkway Separation. Except as required for crosswalks, per subsection 4, below, where a walkway abuts a driveway or street it shall be raised six inches and curbed along the edge of the driveway or street. Alternatively, the City may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway.

(4) Walkway Width and Surface. Walkways, including access ways, shall be constructed of concrete, asphalt, brick or masonry pavers, or other durable surface, as approved by the City Engineer, and not less than five feet wide. Multi-use paths (i.e., designed for shared use by bicyclists and pedestrians) shall be concrete or asphalt. The City may also require six foot wide, or wider, concrete sidewalks in developments where pedestrian traffic warrants walkways wider than five feet.

**SECTION 8
OFF-STREET PARKING AND LOADING**

- 8.1. NEW AND EXISTING FACILITIES TO PROVIDE PARKING AND LOADING**
- 8.2. REDUCTION OF REQUIRED PARKING AREA PROHIBITED**
- 8.3. REDUCTION OF PARKING AND LOADING AREA ALLOWED IN THE BUSINESS CENTER**
- 8.4. LOCATION**
- 8.5. JOINT USE**
- 8.6. OFF-STREET AUTOMOBILE PARKING REQUIREMENTS**
- 8.7. OFF-STREET LOADING REQUIREMENTS**
- 8.8. PARKING AND LOADING AREA DEVELOPMENT REQUIREMENTS**
- 8.9. GENERAL PROVISIONS, OFF-STREET PARKING AND LOADING**
- 8.10. HANDICAPPED PARKING**
- 8.11. ACCESS TO STATE HIGHWAYS**
- 8.12 BICYCLE PARKING REQUIREMENTS**

8.3 REDUCTION OF PARKING AND LOADING AREA ALLOWED IN THE DOWNTOWN CORE OVERLAY ZONE BUSINESS CENTER

Off-street parking and off-street loading area requirements for a particular use as enumerated in this Ordinance are not required for a new or expanding use when located within the Parking District delineated in Exhibit "A" - Downtown Core Zone Overlay District except as specified below.

- (a) New residential development is required to meet the minimum off-street parking requirements in Section 8.6 except that in order to encourage the preservation and reuse of historic buildings, no parking shall be required for new or existing multifamily units above the ground floor in buildings built before 1950. Where off-street parking is required:
 - (1) A 25% reduction in the minimum number of parking spaces required is allowed for age-restricted senior housing; and
 - (2) Required parking may be permitted on another lot within 500 feet of the subject property with a shared parking agreement or other proof of legal parking access.
- (b) New non-residential buildings over 4,000 sf in gross floor area or which have more than 10 employees (based on maximum number of employees who will be at the site at one time, either on a single shift or an overlap of shifts) are required to provide a minimum of 50% of the off-street parking spaces required by Section 8.6.

Exhibit A



8.6 OFF-STREET PARKING

Industrial Uses

(o) ~~Storage~~ Manufacturing/Warehouse; 0-49,999 square feet of floor area

One (1) space per 5,000 square feet or one (1) space per employee, whichever is greater

50,000-99,000 square feet of floor area

One (1) space per 10,000 square feet or one (1) space

~~(p) Manufacturing Establishment with~~ 100,000 or more square feet of floor area

One (1) space per 15,000 square feet or one (1) space per employee, whichever is greater

(p) (a) Wholesale Establishment

One (1) space per employee or 1,000 square feet of gross floor area, whichever is greater, plus one (1) space per 700 square feet of patron-serving area

**SECTION 11
SIGN PROVISIONS**

- 11.1 SIGN REGULATION PURPOSE**
- 11.2 DEFINITIONS RELATING TO SIGNS**
- 11.3 GENERAL LIMITATIONS**
- 11.4 SIGNS GENERALLY PERMITTED**
- 11.5 PROHIBITED SIGNS**
- 11.6 PERMIT REQUIREMENTS**
- 11.7 DESIGN REVIEW REQUIREMENTS**
- 11.8 PERMIT APPROVAL OR DENIAL**
- 11.9 RESIDENTIAL SIGN REGULATIONS**
- 11.10 COMMERCIAL AND INDUSTRIAL SIGN REGULATIONS**
- 11.11 NONCONFORMING SIGNS**
- 11.12 ENFORCEMENT OF SIGN ORDINANCE**
- 11.13 SPECIAL CATEGORY SIGNS**
- 11.14 CONSTRUCTION AND MAINTENANCE STANDARDS**
- 11.15 VARIANCES**
- 11.16 ILLUMINATED SIGNS**
- 11.17 SIGN REMOVAL**

11.6 PERMIT REQUIREMENTS.

(e) The application for a sign permit shall be accompanied by the following plans and other information.

(1) The names and address of the sign company, person authorizing erection of the sign and the owner of the subject property.

(2) The location by street address of the proposed sign structure.

~~(3) A statement of valuation of the sign.~~

11.7 Design Review Requirements

All signs permitted within the Commercial General (CG) or Residential Commercial (RC) zones of the City shall conform to the following design review criteria, unless otherwise provided for in this Ordinance:

(b) The following styles of lettering shall be required unless other lettering is approved by the ~~Design Review Committee~~ Design Review Board: Black Chancery, Gothic (Old English), Fraktur, Frank Normal (True Type), Parchment (True Type), Textura, Valiant, Vivaldi Italic (True Type), Kunstler, Clairvaux, San Marco, Blackletter, Modern Blackletter, ~~or~~ Modern Chancery Medici Script Std (adobe.com and myfonts.com), Blackletter 686 (myfonts.com), Frakto Regular (myfonts.com), Wilhelm Klingspor Gotisch (myfonts.com), Wedding Text (myfonts.com), Magdeburg (myfonts.com), Stuttgart Gothic (myfonts.com), Albrecht Durer Gothic (myfonts.com), Fleischmann Gotisch PT (myfonts.com), Textur Gotisch DFR (myfonts.com), Francesca Gothic (myfonts.com), Richmond Fraktur Regular (myfonts.com), Dala Text (myfonts.com), Kings Quest (myfonts.com), (Fette) Gotisch EF Bold (myfonts.com), Jessen Schrift D (myfonts.com), Royal Bavarian Plain and Fancy (myfonts.com), DS-Andreas-Schrift (fraktur.com), DS-Caslon-Gotisch (fraktur.com), DS-Claudius (fraktur.com), DS-Fette Gotisch (fraktur.com), DS-Gutenberg (fraktur.com), DS-Wilhelm-Klingspor-Schrift (fraktur.com), DS-Lincoln-Gotisch (fraktur.com), DS-Maximilian (fraktur.com), DS-Maximilian-Zierbuchstaben (fraktur.com), DS-Peter-Jessen-Schrift (fraktur.com), DS-Ratdolt-Rotunda (fraktur.com), DS-Strallburg (fraktur.com), DS-Tannenberg (fraktur.com), DS-Wallau (fraktur.com), DS-Weill-Gotisch (fraktur.com), DS-Alte Schwabacher (fraktur.com), DS-Ehmcke-Schwabacher (fraktur.com), DS-Offenbacher Schwabacher (fraktur.com), DS-Offenbacher Schwabacher Zusatzpaket (fraktur.com)

(Copies of the lettering styles are available from the City Recorder.)

11.13 Special Category Signs

(j) Mural Signs. In addition to the allowable sign area, mural signs are allowed subject to prior approval of the ~~Design Review Committee~~ Design Review Board. Upon application, the ~~Design Review Committee~~ Design Review Board may authorize such mural signs upon a finding that the design and placement of the proposed sign contributes to the Bavarian Theme District, if applicable. The ~~Design Review Committee~~ Design Review Board may also authorize specific placement of such mural signs off-premises or in a manner, which exceeds the applicable size or height limits prescribed herein. The findings of the ~~Design Review Committee~~ Design Review Board shall be based upon specific standards adopted by the committee and shall be submitted in writing to the City Manager or designee who shall attach same to the application for sign permit.

**SECTION 14
INFILL DEVELOPMENT OVERLAY ZONE**

14.1. PURPOSE

14.2. APPLICABILITY

14.3. MINIMUM INFILL DEVELOPMENT POTENTIAL REQUIREMENTS (80 PERCENT RULE)

14.4. INCENTIVE LAND DIVISION STANDARDS

14.5. STREET, ACCESS AND PEDESTRIAN WAY STANDARDS

~~14.6. RESIDENTIAL DEVELOPMENT STANDARDS~~

~~14.7. ADDITIONAL MULTI-FAMILY RESIDENTIAL DEVELOPMENT STANDARDS~~

14.1 PURPOSE

The purpose of the Infill Development Overlay Zone is to foster residential development in specific established neighborhood areas in order to achieve the following community objectives: reduction of pressure to expand the community's Urban Growth Boundary (UGB), more efficient use of existing infrastructure and services (i.e., streets, water, sewer, solid waste disposal), provision of affordable housing, and avoidance of secondary growth related to urban sprawl. Although development densities are based on the underlying land use zoning, the Infill Development Overlay Zone applies specific standards that encourage compatible development on vacant, underutilized, or partially used land.

14.2 APPLICABILITY

This Ordinance shall apply to all land within the City's Infill Development Overlay Zone as shown in Exhibit A.

14.3 MINIMUM INFILL DEVELOPMENT POTENTIAL REQUIREMENT (80 PERCENT RULE)

(a) In order to be eligible for Incentive Land Division Standards (Section 14.4) and Incentive Street Design Standards (Section 14.5(a)), a proposed development must demonstrate that it will yield residential density of at least 80 percent of the maximum density allowed by the Comprehensive Plan and Map for the underlying zone.

(b) A development proposal may satisfy the 80 Percent Rule by submitting a hypothetical future development plan that meets the density criterion in (a), and recording "no-build" easements for all areas of the property to be reserved for future construction based on that submitted plan.

(c) The "no-build" easement shall be eliminated when development in compliance with the 80 Percent Rule occurs.

(d) The "no-build" easement shall be prepared and recorded using a form approved by the Mt. Angel City Attorney.

14.4 INCENTIVE LAND DIVISION STANDARDS

The following standards, which are intended to allow greater flexibility in lot size requirements for development meeting the 80 Percent Rule, shall apply within the Infill Development Overlay Zone. Except as specifically provided in this Section, the standards and requirements of the underlying zoning, other Sections of this Ordinance and the Subdivision Ordinance shall apply.

(a) Proposed land divisions meeting the 80 Percent Rule shall be eligible for review subject to the specific standards and requirements in this Section.

(b) For land divisions (subdivision or partition) ~~which meet the 80 Percent Rule,~~ minimum lot size requirements may be reduced up to a maximum of 20% based on dedications made in conjunction with the land division action. Such dedications may be for public right-of-way or for permanent parks and open space. Reductions to lot sizes shall be permitted on a one-for-one basis, up to the total square footage of dedications. ~~No parcel shall be reduced to less than 80 percent of the minimum parcel size established by the standards of the underlying zone through the application of this provision.~~ Text or a table describing the allocation of right-of-way area credits among platted lots shall be recorded as part of the plat drawings.

~~(c) A proposal for immediate construction at less than 80 percent of the maximum permitted density in the Comprehensive Plan and Map for the underlying zone shall be eligible for the provisions of this Section if it complies with the 80 Percent Rule provisions of Section 14.3 above. To ensure that structures will not be constructed in locations that conflict with future infill development, the "no build" easement shall cover all portions of the subject property which must be reserved for future development in order to meet the 80 Percent Rule, including future streets and access easement areas.~~

14.5 STREET, ACCESS, AND PEDESTRIAN WAY STANDARDS

The following standards shall apply within the Infill Development Overlay Zone. Except as specifically provided in this Section, the standards and requirements of the underlying zoning, other Sections of this Ordinance, and the Subdivision Ordinance, shall apply:

(a) Incentive Street Design Standards. Infill development meeting the 80 Percent Rule shall comply with the street, and private accessway standards set forth in Exhibit B, which are intended to allow greater flexibility in access width requirements for development.

(b) Connectivity. Except at locations where connectivity is precluded by environmental or topographic constraints or by existing development patterns, streets and private accessways shall be designed to extend through the lot being served and abut adjoining property or streets, creating the opportunity to form a connected public access network. Private accessways, and access drives shall be covered by public access easements in a form approved by the Mt. Angel City Attorney. Cul-de-sacs, with maximum length not to exceed 400 feet, may only be allowed at locations where connectivity is precluded by environmental or topographic constraints or by existing development patterns.

(c) Street Trees and Landscaping. On arterial, collector, and standard local streets, a planter strip with street trees and landscaping is required between the street and sidewalk. On all streets and private accessways, a minimum of one (1) street tree shall be provided for each 35

feet of public or private street frontage, or fraction thereof. Street trees shall be equally spaced to the maximum extent possible. A five (5)-foot-wide street landscaping easement shall be located immediately adjacent to the public access easements for public and private accessways.

(d) Pedestrian Ways. Where a block is greater than 400 feet in length, a pedestrian way through the block, connecting with adjoining development, streets, or accesses shall be provided. Where a single-outlet access is necessary (i.e., a cul-de-sac, private accessway, or access drive that cannot make a through connection in the future due to constraints), a pedestrian way connecting the single-outlet access with adjoining development, streets, or accesses shall be provided. Pedestrian ways shall have a minimum five (5)-foot-wide, paved, all-weather surface within a minimum ten (10)-foot-wide easement or tract.

(e) Lighting. Pedestrian-scale lighting shall be required as part of construction of infill local streets, private accessways, access drives, and pedestrian ways extending more than 220 feet between intersections with other transportation network elements.

(f) Curb Radii. To reduce pedestrian crossing distances and slow traffic, the curb radius for local streets and accessways shall be no greater than 20 feet and no less than 15 feet.

(g) On-street Parking. On-street parallel parking should be provided on collector and local streets.

~~14.6 RESIDENTIAL DEVELOPMENT STANDARDS~~

~~The following standards which are intended to ensure that infill development is compatible with existing buildings and neighborhoods shall apply within the Infill Development Overlay Zone, superseding other provisions of this Ordinance. Except as specifically provided in this Section, the standards and requirements of the underlying zoning and other Sections of this Ordinance shall apply.~~

~~(a) Review Process for Single Family and Two Family Dwellings. Single family and two family dwellings shall meet the standards of this Section and shall be subject to review by the Design Review Committee. Appeals of the Administrator's decision shall be to the Planning Commission.~~

~~(b) Building Orientation. New residential buildings shall have their primary orientation towards the street and shall incorporate features such as front porches, windows, doorways, and paved walkways that connect to sidewalks (or shared accessways). Unless the curvature of the street makes it impractical, the primary residential structure shall be oriented so that the front building line is parallel to the abutting street. Where Mt. Angel Development Regulations Page 14-4 public street frontage is not provided and lots are served by a private accessway, the fronts of residential buildings shall be oriented to the private accessway.~~

~~(c) Garage Location. Garages shall be set back behind the front facade of the house by at least five feet. For single family and duplex uses, three car garages are permitted only when the garage door for the third car is set back from the front wall plane of the main garage by at least~~

~~two (2) feet. A conditional use permit is required for garages accommodating more than three cars on a single-family or duplex lot.~~

~~(d) Front Porches. New residential buildings shall have a usable covered, but not enclosed, outdoor front porch. Porch dimensions shall be a minimum of six (6) feet in depth and a minimum of eight (8) feet in length. Porches shall have a gable, hip, or shed roof, shall be supported by boxed or round columns, and shall have a perimeter railing.~~

~~(e) Windows. Windows shall be oriented vertically, with proportions of at least 3:2. Large horizontally oriented "picture windows" are not permitted; a series of vertical windows may be used instead.~~

~~(f) Trim and Details. Trim shall be used around the windows, doors, frieze, and corners of buildings. Details shall be used around the porch, fascia board, and window and door tops.~~

~~(g) Roofs. Hip and gable roofs with a minimum pitch of 4/12 shall be provided where an abutting property includes a main structure with a minimum roof pitch of 4/12. In all other cases, a minimum pitch of 3/12 shall be provided. Jerkinhead and gambrel roofs can also be used. Each roof shall incorporate a hip, shed, eyebrow, or gable dormer in its roof design.~~

~~(h) Parking Location. With the exception of driveway parking, off-street parking areas and parking lots shall not be located in the front yard.~~

~~(i) Yards. Front and side yards that abut the street shall be visually open to the street. In these areas, fences and hedges shall be less than four (4) feet in height.~~

~~(j) Fences/Walls. Fences and walls in front yards and corner side yards shall be no more than four (4) feet in height and shall not be solid in design. Along rear and interior yard lines, walls may be solid and may be up to six (6) feet in height, except in front and side yards that abut a street or public access easement, where wall height shall be less than four (4) feet within ten (10) feet of the street or public access easement.~~

~~(k) Development Pattern. Except as may be permitted through the planned unit development process, no more than four identical structures in a row shall be allowed.~~

~~(l) Front Yard Setback. The minimum front yard setback shall be 15 feet, measured from the edge of the right-of-way or public easement line to the front of the structure. The minimum setback for garages shall be 20 feet from the edge of the right-of-way or public easement line.~~

~~(m) Dwelling Height. The maximum height permitted for a new dwelling on an existing lot, or lots created through a partition, shall be 20 feet or the average height of the dwellings on all abutting parcels in the same underlying zoning district, whichever is higher. The Planning Commission may approve a taller dwelling through a conditional use process.~~

14.7 ADDITIONAL MULTIFAMILY RESIDENTIAL DEVELOPMENT STANDARDS

The following standards, which are intended to ensure that infill development is compatible with existing buildings and neighborhoods, shall apply within the Infill Development Overlay Zone, superseding other provisions of this Ordinance. Except as specifically provided in this Section, the standards and requirements of the underlying zone and other Sections of this Ordinance shall apply.

- ~~(a) Review Process for Multifamily and Attached Dwellings. Multifamily and attached dwellings shall meet the standards of this Section, and shall be subject to review by the Design Review Board in accordance with the procedures set forth in Ordinance 617.~~
- ~~(b) Scale. Buildings with walls greater than 80 feet in length shall include street facades that are varied and articulated at regular 20, 30, 40, or 50 foot intervals along the facade to provide the appearance of smaller buildings. Articulation shall be achieved through use of offsets, jog, variation of finishes, projections, entries, or other forms of distinctive changes.~~
- ~~(c) Front Facades. All primary, ground floor common entries or individual unit entries of street frontage units shall be oriented to the street, not to the building interior or to a parking lot. The front elevation of large structures must be divided into smaller areas or planes of 500 square feet or less. Projecting features such as porches, balconies, bays, dormer windows, and roof pediments are encouraged for structures facing a street.~~
- ~~(d) Main Entrance. Primary structures must be oriented with their main entrance facing the primary street frontage of the site. If the site is on a corner it may have its main entrance oriented to either street or to the corner.~~
- ~~(e) Unit Definition. Emphasize each dwelling unit by including a roof dormer or bay windows on the street facing elevation, or by providing a roof gable or porch that faces the street.~~
- ~~(f) Roof Lines. Hipped or gabled roofs are recommended. Roof line offsets shall be provided at intervals of 100 feet or less to create variety in the massing of structures and relieve the effect of a single, long roof. Roof line offsets shall be a minimum eight (8) foot variation either vertically from the gutter line or horizontally.~~
- ~~(g) Parking. Parking and loading areas may not be located between the primary structure and right-of-way on which the structure fronts. If there is no alley and motor vehicle access is from the street, parking must be either in a garage that is attached to the primary structure, in a detached accessory structure located at least 50 feet from the front property line, or in a parking area at the side or rear of the site.~~
- ~~(h) Parking Lot Landscaping. Where more than four surface parking spaces are provided abutting a residential district or street, a minimum five (5) foot wide landscaped yard shall be established between the parking lot and abutting residential district or street. The landscaped yard area shall be planted with a continuous three (3) foot high hedge that will reach maturity within five (5) years.~~
- ~~(i) Screening. All mechanical, electrical, communications, and service equipment shall be screened from public view by parapets, walls, fences, landscaping, or other suitable means.~~

Exhibit A Infill Development Overlay Zone

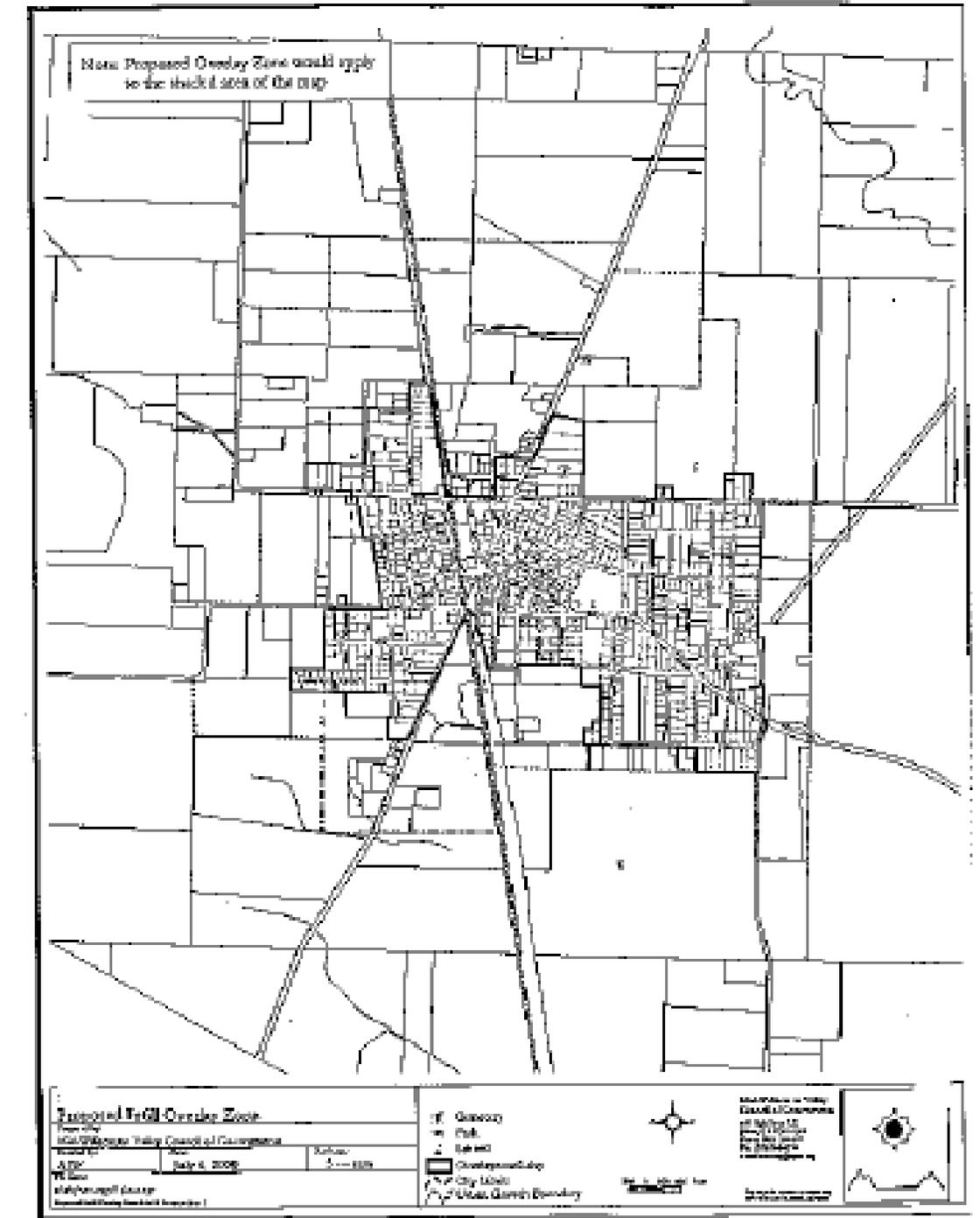


Exhibit B

Street and Accessway Standards

Street Type	Pavement Width	Travel Lane	On-Street Parking	Minimum R.O.W.	Sidewalk	Park Strip	Street Trees	Average Daily Trips
Standard Local Street	30-34 ft ¹	2	2 sides	50 ft. – 55 ft. (up to 75 lots)	5 ft. min. both sides	7.5 ft. min.	yes	250-750
Infill Local Street	28 ft.	2	1 side	35 ft. – 40 ft. (up to 25 lots)	5 ft. min. one side	not required	yes in easements	< 250
Private Accessway	18 ft.	2	no	20 ft. easement (up to 4 lots)	none	none	yes in easements	< 40

¹ Narrow Street Option (30 foot pavement width)

The Planning Commission shall allow use of the narrow street option for local streets if all of the following conditions are met:

1. Not more the 600 feet in the block or street segment
2. Access for not more than 20 dwelling units on the block or street segment.
3. Only permitted on streets without significant through traffic, including but not limited to, cul-de-sacs.
4. No curves are present that would create sight-distance problems.
5. No other problems are present that would interfere with the proper functioning of a narrow street.

**SECTION 16
DESIGN REVIEW BOARD**

16.1. PURPOSE

16.2. MEMBERSHIP

16.3. OFFICERS

16.4. REVIEW AUTHORITY

16.5. PROCEDURES

16.6. APPEALS

16.1 Purpose

A Design Review Board is hereby created and established to promote the general public welfare and to assist the Planning Commission in enhancing, protecting and preserving aesthetic appeal and beauty of the City. The Board serves as an advisory board to City staff or the Planning Commission.

16.4 Review Authority

The Design Review Board shall review:

- (a) All Bavarian Theme District applications per Section 15.
- (b) All Sign Permits under Section 18 of the Zoning Ordinance.
- ~~(c) The Design Review Board shall review and approve all permit applications for the building or location of structures subject to Section 1.10(b)(manufactured home) and Section 6.1(f) of the Zoning Ordinance for conformance to those applicable Section requirements.~~
- ~~(d)~~ May review other design issues referred to the Board by the Planning Commission.

16.5 Procedures

Upon receipt of an application requiring review by the Design Review Board the City Recorder shall:

- (a) Within 72 hours notify the Board of the receipt of application and either provide copies to all Board members or indicate where copies are available for viewing by the Board members.
- (b) Each Board member shall within 72 hours notify the City Recorder of either approval of the design as submitted or request a full Board meeting on the application.
- (c) A Board meeting shall be scheduled in accordance with applicable law and the adopted rules of procedure of the Board.
- (d) For Type I Reviews, the Board advises the City staff planner.
- (e) For Type II and III reviews, the Board advises the Planning Commission.

16.6 Appeals

~~An appeal by an aggrieved party must be filed with the Recorder within ten (10) days of the date of mailing of the Notice of Decision of the Design Review Board to the Planning Commission. The appeal must be files in writing and state wherein the board failed to conform to provisions of the applicable ordinances.~~



Mt. Angel

Bavarian District Planning and Design

The PDF version of this report has [hypertext links](#). Click on the [page number](#) to return to the [Table of Contents](#). Click on the [Index](#) to go to individual topics. All dark blue text is [hypertext](#).



Oktoberfest Joy
by Jerry Joslin

Mt. Angel Bavarian District Planning and Design



City of Mt. Angel, Oregon



University of Oregon
Planning, Public Policy & Management Department
“Green Cities”



Oregon Department of Land Conservation &
Development

September 21, 2014 **Draft**

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TABLE OF CONTENTS

Acknowledgements..... 4

Table of Contents 6

Introduction 7

Culture

 Catholicism10

 Germanic12

 Hispanic17

 Agricultural.....19

 Events21

 Recommendations..... 22

Environmental Design

 Urban Design & Public Space 24

 Architecture & Landscape.....28

 Biophilic Design & Urban Agriculture 30

 Public Art & Experiential Design..... 35

 Recommendations..... 37

Infrastructure

 Walkability41

 Multi-modal Transportation 43

 Resources46

 Energy48

 Recommendations..... 50

References..... 53

Index69

Figures

 Mt. Angel Location & Vicinity Maps..... 8

 Mt. Angel Downtown Aerial Map 9

 Bavarian Theme District Overlay Zone16

 Bavarian Theme District Development Plan..... 26

 Urban Design & Public Space Plan..... 27

 Biophilic Design & Urban Agriculture Plan 34

 Multi-Modal Transportation Plan 45

 Design Charrette Program66

 Design Charrette Map 67

 Local Bicycle Tour.....68

 Group Photo 70

INTRODUCTION

Mt. Angel has an extraordinary cultural heritage based on Germanic ancestry and Catholicism. This heritage is blended with contemporary Oregonian agriculture and an emerging Latino community. Student researchers from the **University of Oregon “Green Cities”** course researched sustainable development, urban resiliency and regeneration with a particular focus on how these concepts could help Mt. Angel create a more vibrant sense of community and place.

This report summarizes this program and synthesizes the concepts that “tell the story” of a dynamic City of Mt. Angel.

In addition to studying new urbanism concepts, the researchers conducted a public design charrette; collaborated with local and international planning and design experts; and prepared a variety of presentation media, some of which have been incorporated in a project video.

This program was initiated by Oregon Department of Land Conservation and Development Willamette Valley Representative **Angela Lazarean** and Mt. Angel City Administrator **Eileen Stein**. University of Oregon Adjunct Instructor **Ric Stephens** coordinated student research and media.

Students from the **Mentor and Student Research Lab** at the Gdańsk University of Technology, Poland, provided expertise in “Designing Public Space.”

City Planner **Stefan Netsch** from the Karlsruhe Institute of Technology, shared “Planning Principles in Rural Areas in Germany.”

Oregon Department of Land Conservation and Development staff **Tom Hogue** and **Karen Swirsky** provided support for economic development and transportation planning.

Nils Eddy and **June Stephens** assisted with project photography and videography.

Catering for the design charrette was provided by the Glockenspiel Restaurant and assisted by City volunteers.

City of Mt. Angel Mayor **Andrew Otte** graciously hosted the design charrette.

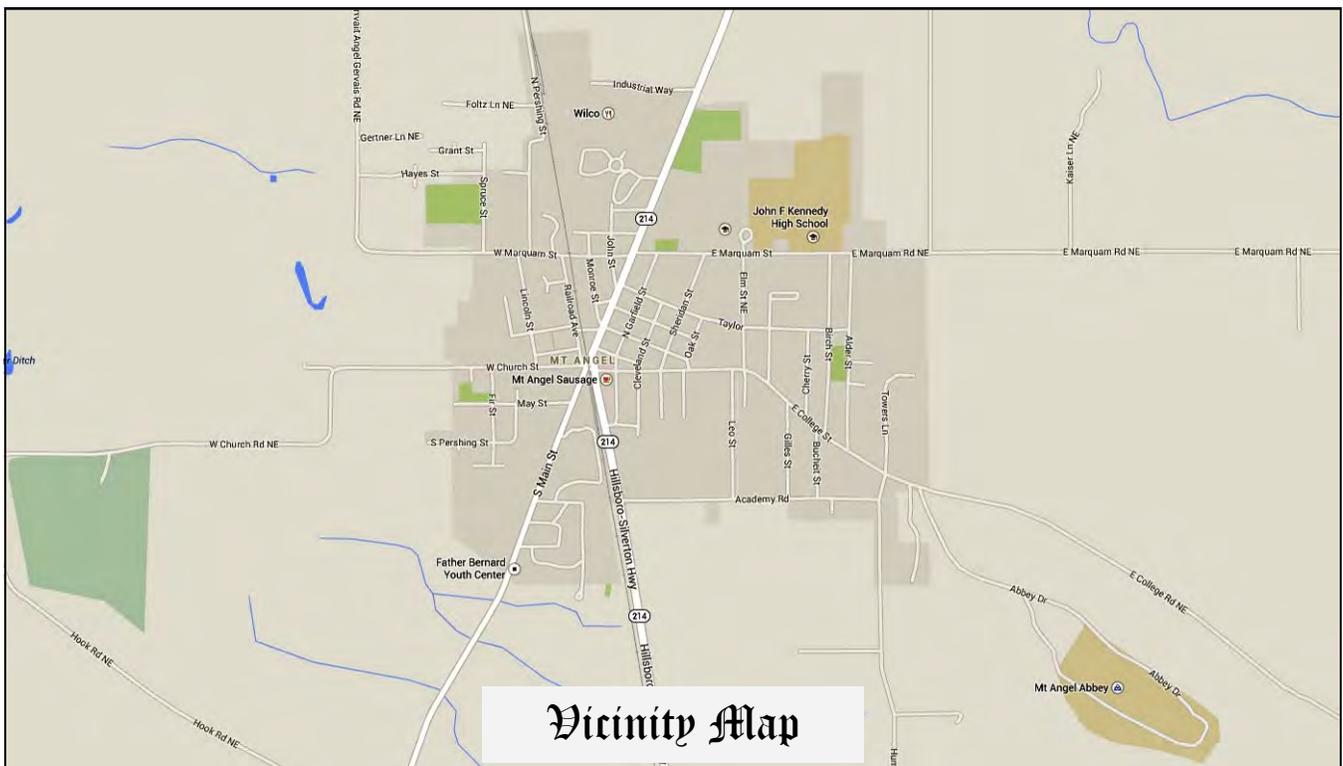
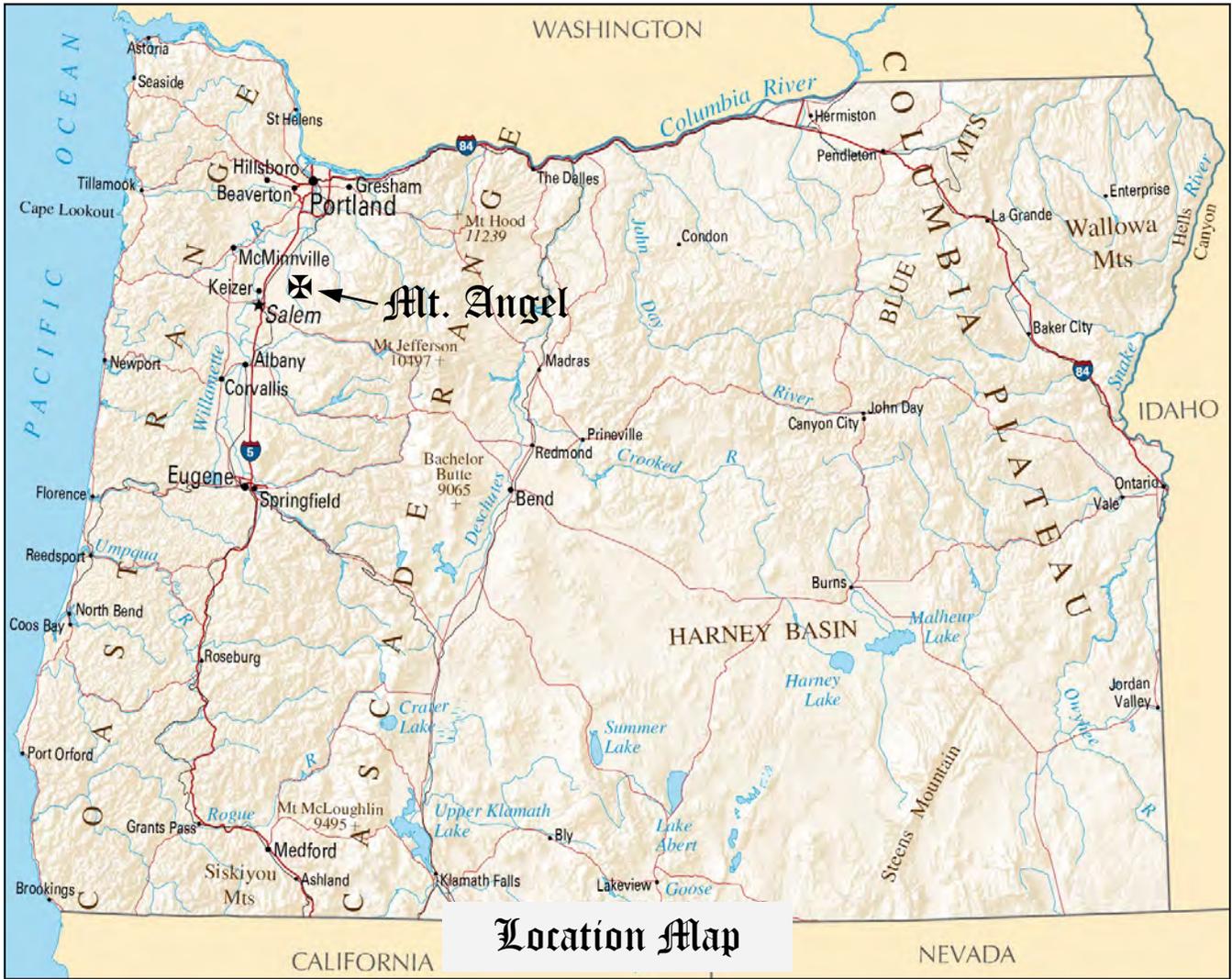
Mt. Angel citizens and public officials engaged with the research team during the course of the design charrette.

This study is dedicated to these participants and the community of Mt. Angel.



Willkommen in Mt. Angel





MT. ANGEL DOWNTOWN AERIAL MAP



CULTURE

CATHOLICISM



Order of Saint Benedict

The story of Mt. Angel begins with the founding of an abbey in 1881 by Benedictine monks from Engelberg, Switzerland.

“The community of Mount Angel was named in 1883 by the Rev. Father Adelhelm Odermatt, O.S.B., in compliment to

Engelberg [Angel Mountain], Switzerland.” (McArthur, 2003)

Father Adelhelm was searching for a location to build a new chapel and selected Lone Butte for its unique geographic features and location. Even before the construction of the chapel, the butte had a spiritual significance to the Kalapuyan Indians and was named *Tapalamaho* which means “Mount of Communion.”

The nearby town and post office were named *Roy* in 1850, and the railway station was named *Fillmore* after an official with the railroad in 1881. The town and railway station were renamed *Mt. Angel*; and the post office and Lone Butte were renamed *Mount Angel* in 1883 coinciding with the construction of a pilgrimage chapel on the summit of the butte. A year later, Mount Angel Abbey, a Benedictine monastery, was also moved to the summit. In 1889 the abbey

opened a seminary for training priests. Within the city limits, Mt. Angel is home to the Queen of Angels Monastery run by the Benedictine Sisters of Mt. Angel. Saint Mary Catholic Church is one of the largest in the region, and both historic structures are listed in the National Register of Historic Places.

The historic city Catholicism was determined by the founding monks and was supported with the religious orientation of the pioneer Bavarian settlers. It continues to have a dominant religious influence with the expansion of the abbey, monasteries, churches and schools.

The emerging Latino population is also predominantly Catholic, and this demographic



Mount Angel Abbey

shift offers a unique opportunity to unify the Germanic and Hispanic cultures. There are numerous Catholic holidays and events that can be celebrated with Germanic and Hispanic

CULTURE CATHOLICISM



Engelberg Coat of Arms

traditions. [See [Culture Events](#)]

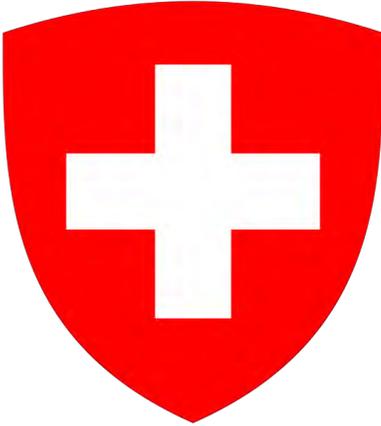
Beginning in the 1970s, the Church has provided a unique forum for civic engagement, and has initiated numerous programs such as Spanish-language Mass and multi-cultural training in the seminary.



St. Mary Catholic Church

CULTURE

GERMANIC



Swiss Coat of Arms

The Benedictine monks who founded Mt. Angel were from Engelberg, a town in the Swiss canton, Obwalden. The town was historically famous for its 12th Century abbey and, during the 19th Century, resort tourism. Both Catholicism and tourism are relevant to contemporary Mt. Angel.

In addition to Catholicism, the Benedictine monks imported Swiss culture in a variety of forms ranging from environmental design to intangible cultural heritage.

At the end of the 19th Century, settlers from the Kingdom of Bavaria began farming the region. The City of Mt. Angel incorporated in 1893 with a population that was largely Roman Catholic Swiss and Bavarian settlers.

There are significant distinctions between Swiss and Bavarian cultures, but their language is Germanic-based, and they observe similar religious and social customs. Mt. Angel has blended these into a “Swiss-German community.” (City of Mt. Angel, 2014)

Bavarian Theme District

The City has formally adopted a 19th century Bavarian theme for the

downtown, Edelweiss Village [See *Bavarian Theme District Map*]

“The general purpose of this ordinance is to promote the economic, educational, cultural and general welfare of the people of Mt. Angel by the provision of policies and procedures to protect the characteristics of a Bavarian Architectural Theme District, and the City of Mt. Angel in general.” (City of Mt. Angel, 2013)

“It is deemed essential that the qualities relating to the design of the City maintain a harmonious outward appearance and function of structures and facilities to promote the preservation of property values for residents, and attraction to tourists; one of these qualities are the continued existence and construction of buildings in the Bavarian Theme which maintains a general harmony as to style, form, color, texture, proportion, material and



Switzerland & Bavaria, Germany

CULTURE

GERMANIC



Bavarian Coat of Arms

Angel, 2013)

landscaping. [sic] It is hereby declared as a matter of public policy that the protection, enhancement, perpetuation and use of improvements and landscape features of special theme character or aesthetic interest or value is a public necessity and is required in the interest of the health, prosperity, safety, and welfare of the people.” (City of Mt.

The Glockenspiel

“The original German word “Glockenspiel” is literally translated into English as “bells play”; a more idiomatic translation might be “musical bells”. In German usage it may be applied to any carillon-sized or chime-sized tower bell instrument which plays music, whether or not it accompanies automated animation. Most commonly, it applies to automatically-played musical instruments rather than to manually-played musical instruments. It does not apply to swinging bells.” (Tower Bells, 2014) The Mt. Angel Glockenspiel is the tallest in North America, and the life-sized animated figures tell the history of the City from the original Indians to today’s Oktoberfest celebration. The Glockenspiel plays four times daily ending with

Architecture

The Bavarian theme is based on pre-20th century Swiss-German village architecture commonly known as *Fachwerk* [half-timbered construction]. City requirements are described in the Mt. Angel Development Code Section 15. [See [Architecture & Landscape](#)]

Other cities with Swiss-German themes have similar ordinances, the most well-known being Leavenworth, Washington. In addition to required design elements, Leavenworth also requires a minimum of three decorative design details which “include, but are not limited to, painted trim, decorative fascia, window treatments, balconies, and murals.” (City of Leavenworth, 2014)



Bavarian Architecture

the song Edelweiss.

Signage

There are two primary types of signage in Swiss-German villages: wayfinding such as house names/numbering on the wall at pedestrian

CULTURE

GERMANIC



Mt. Angel Coat of Arms

level, and business signage which is typically a hanging sign with the name of the business along with supporting graphic elements. These are typically ornamental with shapes and figures associated with the business.

To aid in creating a Bavarian themed district, the City should also adopt contemporary Swiss-German signage such as the sign for “Shared Space” [See [Infrastructure, Recommendations](#)]. Other Swiss-German conventions should be used such as including the metric system along with the imperial system (i.e. kilometers and miles on wayfinding signage) and zebra striping for crosswalks. [See [Walkability](#)]

The Mt. Angel Development Code Section 11.10 specifies Germanic heritage-themed lettering in several basic styles:

- ✘ Black Chancery (e.g. *Black Chancery*)
- ✘ Blackletter (e.g. *Typographer Uncialgotisch*)
- ✘ Fraktur (e.g. *Zentenar Fraktur*)
- ✘ Gothic (e.g. *Old English*)
- ✘ Modern Blackletter (e.g. *Yonkers Regular*)
- ✘ Modern Chancery (e.g. *Vivaldi*)

City Coat of Arms

The Bavarian theme is also evident in the City’s coat of arms which is divided into four quadrants. They represent, clockwise from the top right:

- ✘ St. Mary Catholic Church (Catholicism)
- ✘ Beer and wine (agriculture)
- ✘ Hanging banner, flower basket, and Oktoberfest monument (Bavarian heritage)
- ✘ Accordion, Alpine horn and music (Bavarian folk culture)

Oktoberfest

Oktoberfest is an annual, two-week festival ending on the first Sunday of October. It originated in Munich, Bavaria, in 1810 as a horse race celebrating the wedding of the



Bavarian Window, Flower Box & Hanging Sign

crown prince of Bavaria, later King Louis I (1786–1868). The race was soon combined with the state agricultural fair, and food and drink were offered. In the late 20th century the Munich

CULTURE

GERMANIC



Oktoberfest Emblem

breweries celebrated Oktoberfest by setting up large temporary beer halls, each seating 3,000–5,000 people, and hiring bands to entertain the crowds as they ate and drank.

There are a couple of widely accepted versions of why Oktoberfest is celebrated in September. Even in Munich the bulk

of the festival occurs in September. Oktoberfest is a generic word meaning a HARVEST CELEBRATION. Most fall festivals come at the end of the harvest when the crops are in and

everything sits in the lager house. It is time to celebrate the year's hard work and praise the Creator's bounty with a grand harvest festival. Few harvest festivals in Germany are called OKTOBERFEST. All however, are harvest celebrations. In Mount Angel, Oktoberfest always starts the second Thursday after Labor Day – about when the hop harvest is in. (Mount Angel Oktoberfest, 2014)

The Mount Angel Oktoberfest is one of the largest American celebrations of this “fun fest” and includes:

- ✘ Arts & Crafts
- ✘ Cruz'n Car Show
- ✘ Food & Drink
- ✘ Golf Tournament
- ✘ Oktoberfest Road Race

- ✘ Oregon Marathon
- ✘ Volkswalk
- ✘ Wiener Dog Races
- ✘ Others

The venues correspond to the City's Swiss-German Theme:

- ✘ Alpine Garden
- ✘ Bandstand
- ✘ Biergarten
- ✘ Kindergarten
- ✘ Prosgarten
- ✘ St. Mary Church
- ✘ Weingarten



Black Chancery

Blackletter

Fraktur

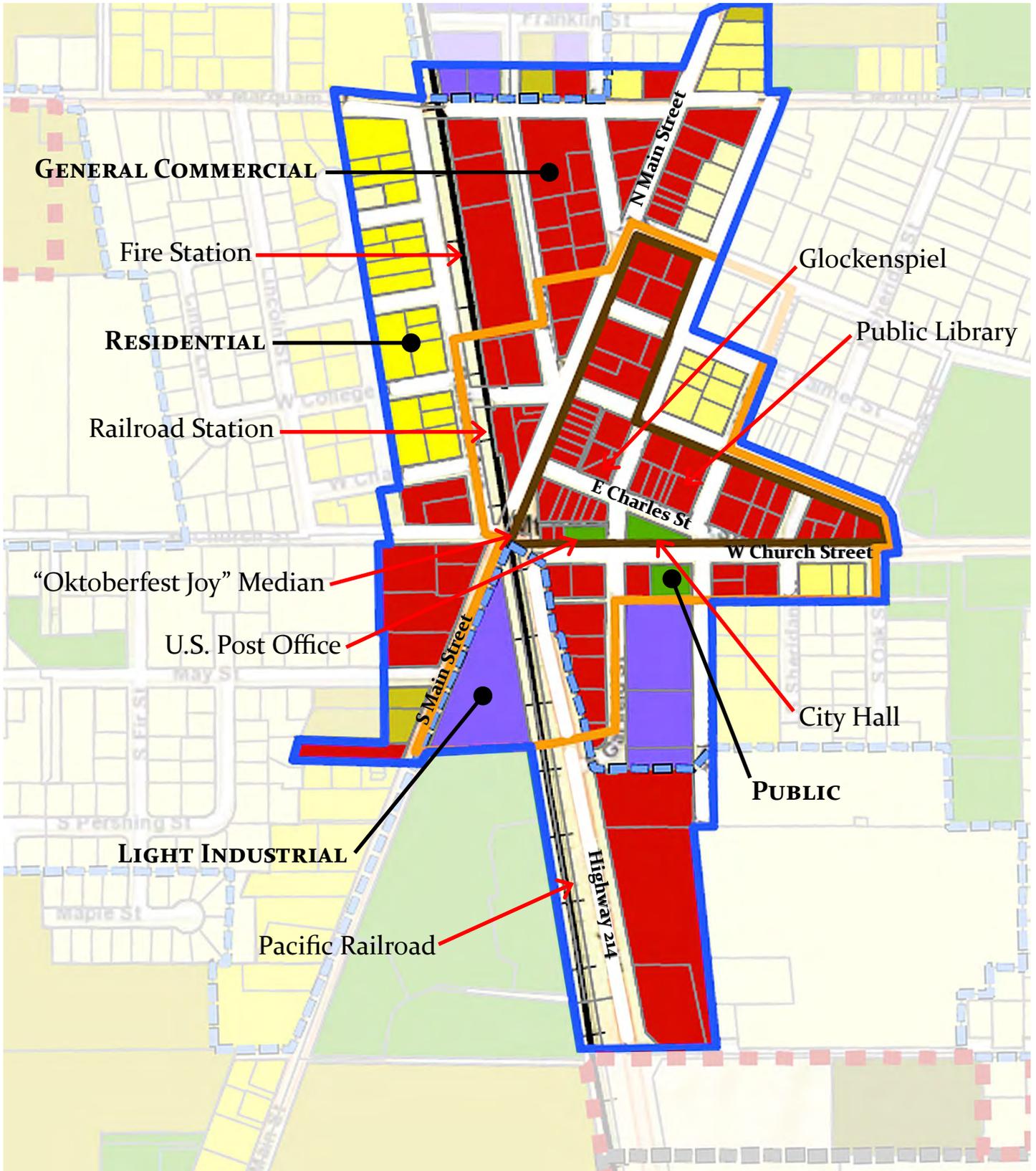
Gothic

Modern Blackletter

Modern Phancery

Bavarian Theme District Sign Lettering

BAVARIAN THEME DISTRICT OVERLAY ZONE



CULTURE

HISPANIC



Latino Country Flags

Through the latter half of the 20th Century, Oregon saw an influx in Latino population associated with growth in the agriculture industry.

United Farm Workers

The emergence of the Latino population's story corresponds to the

influence of the political activism in the 1960s of Cesar Chavez. Founded in 1962 by César Chávez, the United Farm Workers of America is the nation's first successful and largest farm workers union currently active in 10 states. The UFW continues to organize in major agricultural industries across the nation. The UFW signed a contract, also in 2007, with Three Mile Canyon Farms, America's largest dairy-and the first major union contract protecting farm workers in Oregon. They soon signed up another nearby dairy, Willow Creek. The UFW is also pushing its historic bipartisan and broadly backed AgJobs immigration reform bill. (United Farms Workers, 2014)

Numerous issues still face the Latino community:

- ✘ Illegal immigration
- ✘ Lack of employment benefits
- ✘ Housing issues
- ✘ Health concerns
- ✘ Child labor
- ✘ Racism and discrimination

Agriculture is the economic driver of Mt. Angel, and the intersection of cultural heritage with farming is a critical element for the City's sense of place and community. Mt. Angel's future will be shaped by how it adapts to a dynamic shift in demographics and cultural influences.

El Colegio César Chávez

During the early 1970s the Mount Angel College enlisted Sonny Montes to direct their Ethnic Studies Department and recruit Latino students. When the College closed due to financial difficulties in 1973, Mr. Montes formed El Colegio César Chávez—the only Chicano-owned 4-year college in the nation—to educate



Winemaker Jesus Guillén

migrant workers. This was the first college to honor César Chávez during his lifetime, and he visited the college on several occasions. Although the college was instrumental in educating more Latinos than the public

CULTURE

HISPANIC



universities, economic stresses forced the college to close in 1983.

- ✘ Public space (plazas)
- ✘ Murals
- ✘ Blackletter



Demographic Shift

El Colegio Cesar Chavez

Until the mid 20th Century, the population

of Mt. Angel was almost entirely based on Germanic descent. Currently individuals of European descent comprise about two thirds of the population, and a little more than a quarter of the population is Hispanic/Latino. This shift mirrors a similar demographic transition occurring throughout the rural, western United States. There are three key elements associated with the Chicano population:

1. Latinos assist economic growth
2. Cultural diversity makes cities better
3. Mt. Angel's Latino presence is growing

Critical obstacles in realizing these city benefits include:

1. Economic instability
2. Language barrier
3. Spatial segregation
4. Sense of hyper-visibility

These constraints can be mitigated through the recommendations at the end of this section.



Latin Dance

Latino Culture

Several elements of Latino culture coincide with Mt. Angel cultural emphasis:

- ✘ Catholic holidays and events
- ✘ Rural and urban agriculture

CULTURE

AGRICULTURE



Mt. Angel was once surrounded by hop fields and dairy farms, with a creamery that produced the then famous Rose Valley cheese and butter. At one time, a flax plant processed the flax grown in the area. Today, the town is still surrounded by rich farmland growing a variety of

vineyards, flower fields and nursery stock. (Mt. Angel Historical Society, 2014)

Culinary Tourism

Local agriculture includes

- ✘ berries
- ✘ Christmas trees
- ✘ cut flowers
- ✘ filberts
- ✘ flax
- ✘ garlic
- ✘ grapes
- ✘ grass seed
- ✘ hay
- ✘ hops
- ✘ row crops
- ✘ specialty seeds
- ✘ vegetables
- ✘ wheat
- ✘ ...and many others

Farming is one of Mt.

Angel’s cultural elements that can reinforce the City’s heritage and provide a link to the future. The introduction of urban agriculture and community event programming can further this cultural approach to sense of community and place. [See [Biophilic Design and Urban Agriculture](#)]

Agritourism & Culinary Tourism

In addition to the economic value of agricultural produce, there are opportunities to develop agritourism and culinary tourism. These programs can be synchronized with established and proposed events.

[See *Events*]

There are six primary agritourism sectors for Mt. Angel to consider:

- ✘ Farm and ranch recreation
- ✘ Educational experiences
- ✘ Agri-tainment
- ✘ Hospitality services
- ✘ On-farm direct sales
- ✘ Off-the-farm direct sales

(Stewart, 2014)



Vineyards

Travel Oregon targets culinary travelers through its Oregon Bounty campaign. This flagship initiative promotes Oregon as a place where it’s still possible to have authentic, personal encounters with those making and producing Oregon’s award-winning wines, craft beers and other fine artisan food products and ingredients. The campaign promotes Oregon as

CULTURE

AGRICULTURE



Oregon Dept. of Agriculture

a not-to-be missed culinary destination, encourages overnight stays during a typically slower travel season (September-November) and highlights experiences in every region of the state. (Travel Oregon, 2014)

Finally, local agriculture enables Mt. Angel to be more sustainable and resilient.



Flower Fields

CULTURE EVENTS

January

Fastnacht [Carnival], Germany/Switzerland/Austria

February

Wurstfest: A Celebration of German Sausage

<http://www.mtangelwurstfest.com/>

April

Ostern [Easter, German]

La Semana Santa [Easter, Spanish]

May

Maibaum [Maypole Day], Bavaria

June

Festival of Arts and Wine, Mount Angel Abbey

<https://www.mountangelabbey.org/festival-arts-wines/>

Weinfest [Wine Festival, German], Germany/Switzerland/Austria

July

Abbey Bach Festival, Mount Angel Abbey

<https://www.mountangelabbey.org/bach/>

September

Oktoberfest, City of Mt. Angel

<http://www.oktoberfest.org>

Hispanic Heritage Month, September 15-October 15

December

Hazelnut Fest: A German Holiday Market

<http://www.hazelnutfest.com/>

Weihnachten [Christmas, German], Germany/Switzerland/Austria

Navidad [Christmas, Spanish]

CULTURE

RECOMMENDATIONS

Catholicism

1. CULTURALLY SAFE—Select church sites as “culturally safe” spaces for public meetings.
2. FAITH TOURISM—Program tourism development with religious holidays, events and activities.
3. MULTI-CULTURAL PROGRAMS—Develop multi-cultural outreach programs through local churches.
4. PILGRIMAGE—Promote religious “pilgrimage” tourism.
5. PUBLIC SPACE—Incorporate church open spaces for public events and activities.
15. MULTI-CULTURAL EVENTS—Sponsor a multicultural celebration and other events.
16. MULTI-CULTURAL LIAISON—Designate a multi-cultural liaison.
17. PUBLIC SPACE PROGRAMMING—Celebrate Latino presence in Mt. Angel’s downtown public space.
18. TRANSLATOR—Provide a Spanish translator for public meetings/events.
19. UNIVERSITY EXTENSION—Identify and develop a university extension program to replace some of the upper education opportunities lost by the closure of Colegio Cesar Chavez.

Germanic

6. INTANGIBLE CULTURAL HERITAGE—Promote Germanic intangible cultural heritage.
7. SISTER-CITY—Organize a Sister-City or other exchange program with Engelberg and other Swiss and/or German cities.
8. SWISS-GERMAN PROGRAMMING—Celebrate Swiss-German holidays and events. [See [Culture, Events](#)]

Hispanic

9. BILINGUAL MATERIALS—Publish bilingual announcements, FAQs and other city information.
10. COMMUNITY ORGANIZATION—Use community organizing strategies to make a bigger impact.
11. CULTURAL DIVERSITY & BAVARIAN THEME—Integrate cultural diversity into Mt. Angel’s Bavarian Theme story.
12. BILINGUAL WORKSHOPS—Conduct bilingual public workshops.
13. LATINO BUSINESS—Leverage Latino businesses and create a “business incubator” program.
14. LATINO PROGRAMMING—Celebrate Latino holidays and events. [See [Culture, Events](#)]

Agriculture

20. AGRICULTURAL PROGRAMMING—Organize agricultural events and activities. Farmers Fair - Organize an annual Farmers’ Fair to display local agriculture and products. Include floral contests and culinary events.
21. AGRITOURISM—Develop an agritourism program.
24. CITIZEN PARTICIPATION—Create focus groups and/or citizen advisory committees specific to independently initiated projects and these recommendations.
20. CULINARY TOURISM—Develop a culinary tourism program connected to Germanic (Wurst, Wein, Bier) and Hispanic cuisine. Culinary Tourism - Expand promotion of culinary tourism such as the Wurstfest and Wine Festival with others to promote local produce such as Trappist Ale (Mt. Angel Abbey). [See [Culture, Events](#)]
21. ECO-INDUSTRIAL DEVELOPMENT—Use an eco-industrial development model for local agriculture and industry.

Community

24. COMMUNITY ORGANIZATIONS—Encourage development and interaction with

CULTURE

RECOMMENDATIONS

- community organizations such as Chamber of Commerce, Downtown Business Association, Community Gardens, Neighborhood Watch and others.
25. COMMUNITY PROGRAMS COORDINATOR—Appoint or contract with an individual or firm to program city-wide events in collaboration with community organizations and the private sector.
26. CONTACT LIST—Compile a voluntary email list.
27. EDITORIAL MEDIA—Prepare “letters to the editor/guest viewpoints.”
28. EDUCATION—Conduct field trips, city-walks, mapping exercises, workshops and other programs for school children and adults to educate and inform about City heritage. Program downtown landscaping and events for school children. Specialty gardens could be educational projects.
29. LOCAL CURRENCY PROGRAM—Establish a “local currency program” or similar local business economic tool.
30. NEWS MEDIA—Provide press releases.
31. PUBLIC HEARINGS/MEETINGS—Combine public workshops with hearings and publicize the results.
32. PUBLIC SURVEYS—Conduct door-to-door, telephone and public venue information gathering and publish results on the City website / Facebook webpage.
33. PUBLICATION & DISSEMINATION—Provide mailers/direct mail for important announcements and/or city resources.
34. REVITALIZATION PLAN—Create a downtown revitalization plan through a citizens’ advisory committee or other organization.
35. SEASONAL EVENTS—Ice cream social, concerts (high school students), dance lessons, movie nights, street fairs, street performances, library game nights, etc.
36. SOCIAL MEDIA—Initiate a city Facebook webpage.
37. WiFi—Provide free WiFi in the Edelweiss Village.



ENVIRONMENTAL DESIGN

URBAN DESIGN & PUBLIC SPACE



The Bavarian Theme District land use is predominantly General Commercial with some Light Industrial south of Church Street, and small Residential areas along the northwestern and eastern boundaries. [See [Bavarian Theme District Map](#)]

- ✘ **Edges**—The Pacific Railroad tracks provide a traditional east/west urban boundary.
- ✘ **Landmarks**—The Glockenspiel is the central landmark. The “Oktoberfest Joy” fountain is a key transportation landmark, and the St. Mary Catholic Church serves as an eastern landmark. To a lesser degree, the Mt. Angel Railway Station clock and Fire Station also serve as landmarks.
- ✘ **Nodes**—As note, there is not a town square to serve as the principle node. Street closures during Oktoberfest create

Sense of Place

There is an historic sense of place associated with Mt. Angel’s Swiss-German Catholic heritage. This theme has been codified through the Mt. Angel Development Code Bavarian Theme District specific to architectural and signage design aesthetics. [See [Germanic Culture](#)] “Place, unlike space, is described by objects which transmit specific cultural, historical or socially meaningful values which are different for each individual.” (Perec, 1997) The key for Mt. Angel is to aid in “transmitting” these values: telling this story. From the perspective of “imageability,” Mt. Angel has many well-defined elements:

- ✘ **Districts**—The Bavarian Theme District has specific boundaries, and the Edelweiss Village (core downtown) adheres to the District Development Code.
- ✘ **Paths**—Church Street, Main Street and Highway 214 provide a clear framework for the street pattern.



Aerial View of Garfield and Charles Streets

temporary nodes, and the schools and Church provide community nodes. (Lynch, 1960)

Development Pattern

The development pattern for Mt. Angel is based on a unique intersection of three

ENVIRONMENTAL DESIGN

URBAN DESIGN & PUBLIC SPACE

principle roads in the center of the Downtown: East-West Church Street; Northwest-Southeast Railroad Street/Hillsboro-Silverton Highway 214; and Southwest-Northeast Main Street/Hillsboro-Silverton Highway 214. The result is multiple grid orientations and polygonal street blocks. Block sizes in the downtown core are about 200 by 200 feet [~60 x 60m] which is ideal for walking and visual interest.

The Hillsboro-Silverton Highway 214 bisects the District and has an obtuse angle at the center which is marked by a median with the statue “Oktoberfest Joy.” [See [cover photograph](#)] Church Street intersects with the highway, and the Pacific Railroad also travels through this center. This site has the unique ability to be a gateway or entry statement for the downtown core. This establishes a definitive “sense of arrival” which is key to reinforcing sense of place. This intersection is also a key departure from the highway towards the Mt. Angel Abbey.

Within the core downtown, Edelweiss Village, the intersection of East Charles Street and North Garfield Street is another significant center with the Glockenspiel on the northwest corner across from City Hall on the southeast corner. During Oktoberfest, portions of these streets are closed to accommodate street vendors, activities and festival attendees. Converting traditional crosswalk markings to the European style “zebra crossing” with traffic circles will reinforce the Bavarian theme and assist in traffic calming.

Public Space

Mt. Angel has numerous small public spaces such as the gazebo between East Church Street and East Charles Street near City Hall. With the exception of Oktoberfest, these public spaces are largely “unprogrammed,” and most of them

are not improved for public use. As is common for most rural communities, there is no town square or central plaza.

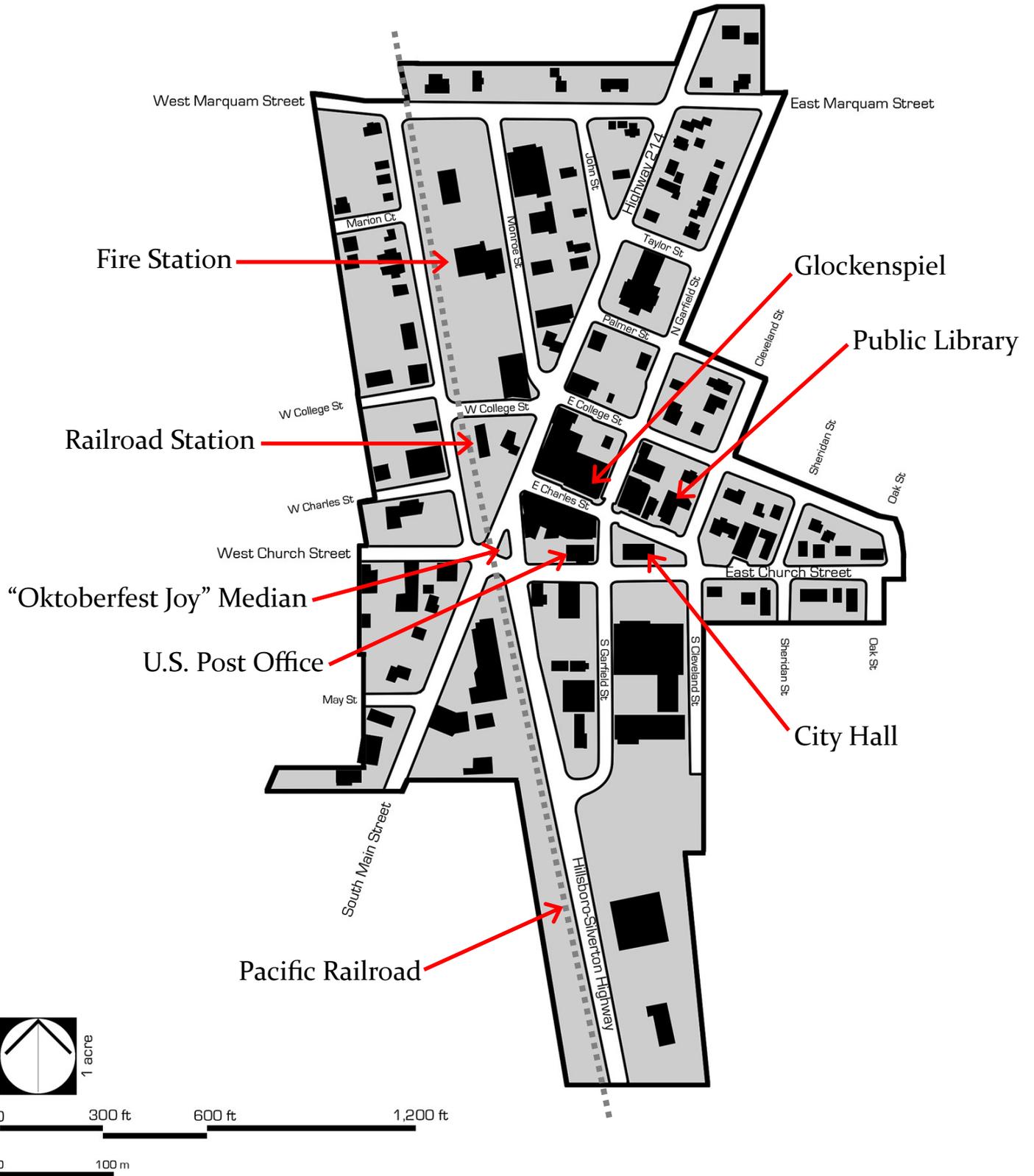
Town Square

In the event, the existing City Hall is removed, this site would serve as an ideal location for a town square. The proposed downtown plaza would provide a needed public gathering space, give focus to downtown core of businesses and provide the impetus for additional redevelopment in downtown. (Crandall Arambula 2008)



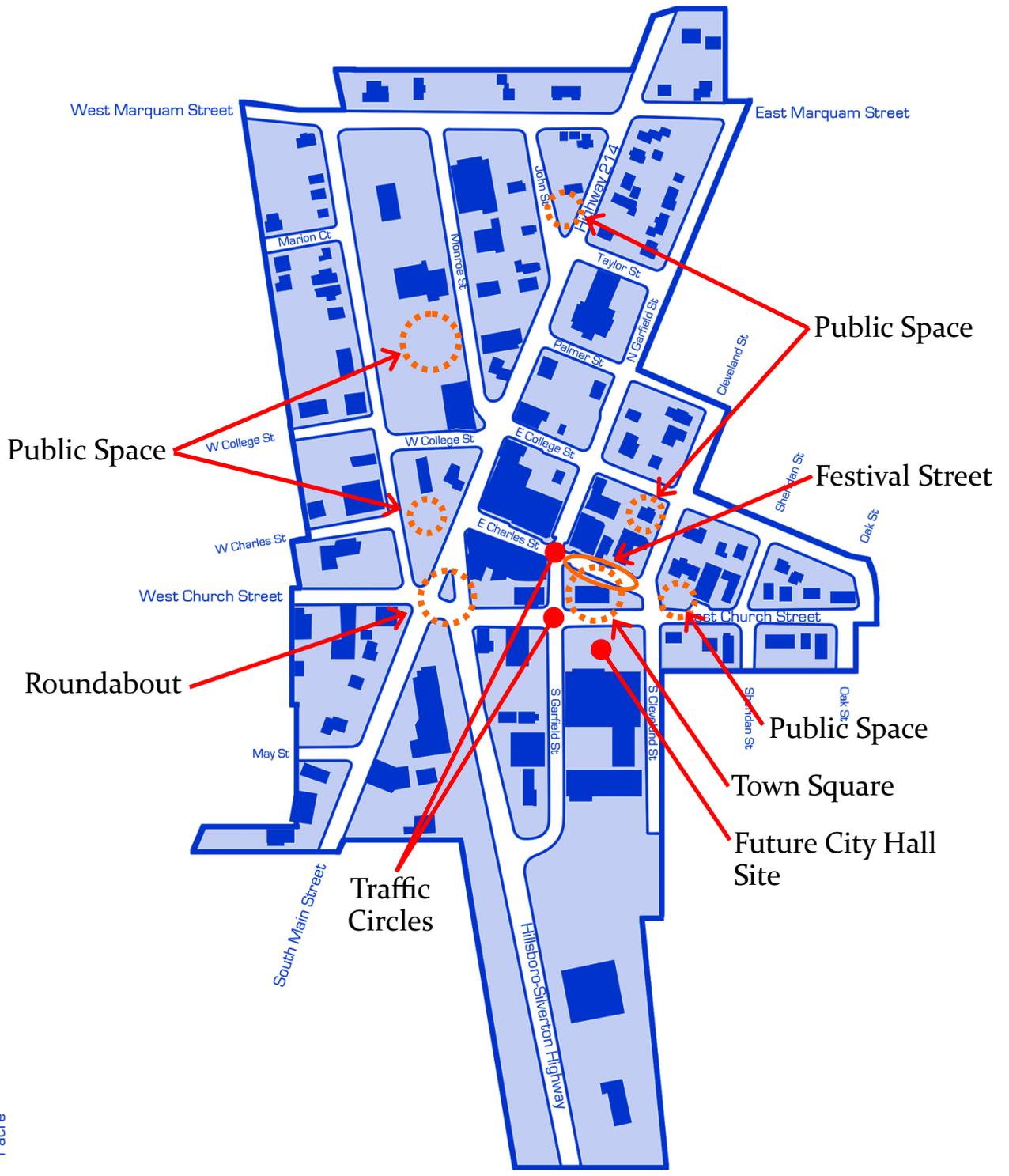
Hanging Sign

BAVARIAN THEME DISTRICT DEVELOPMENT PLAN



BAVARIAN THEME DISTRICT

URBAN DESIGN & PUBLIC SPACE PLAN



0 300 ft 600 ft 1,200 ft

0 100 m

ENVIRONMENTAL DESIGN

ARCHITECTURE & LANDSCAPE



The Edelweiss Village focuses on late 19th century Swiss-German village environmental design. The emphasis is on folk craftsmanship and rural lifestyle design reflected in the architecture and landscaping of typical villages.

- ✘ Balconies or shutters and window flower boxes where second floor windows are included;
- ✘ Architectural balconies not intended for actual use, to have an apparent means of access, i.e., a door or false door, or large window;
- ✘ Use of glazed rather than artificial (painted windows);
- ✘ Roof overhangs for all pitched roofs;
- ✘ Compatibility in materials and consistency in style throughout all elevations on buildings;

Architecture

The City of Mt. Angel Design Review Board determines and approves development when it is consistent with the Bavarian Architectural Theme. There are a variety of references for comparison which essentially reflect late 19th century Germanic architecture with an emphasis on half-timbered construction. This type of architecture has an exposed timber framework with the interstices filled with plaster. The Mt. Angel Development Code Section 15 requires the following Bavarian design features:

- ✘ Inclusion of shutters with windows;
- ✘ Inclusion of window grids to give the look of paned windows;
- ✘ Use of stucco rather than stucco board on large exposed wall. Where stucco board is approved, cover joints with battens;
- ✘ Decorative, protective end caps or scroll-sawed ends on exposed roof overhang support beams;
- ✘ Require rain gutters, down spouts for all eaves, no drainage permitted onto sidewalks, connection to storm sewer where available;
- ✘ Limiting the service windows opening onto sidewalks or public right-of-way. Require at least eight-foot setback for service windows



The Glockenspiel Tower & Restaurant

ENVIRONMENTAL DESIGN

ARCHITECTURE & LANDSCAPE

- to outdoor, privately owned staging area;
- ✘ Avoid partial pitched roofs where visible to the street, to prevent a “false front” look;
- ✘ Battens over six inches (6”) in width should have a thickness of one and one-half inches (1.5”) or more so as to minimize the chances of “cupping” and warping;
- ✘ Use of flower boxes on balconies;
- ✘ Inclusion of decorative scroll work on fascia board and other trim;
- ✘ Inclusion of murals or art work on exterior walls or around windows and doors. (City of Mt. Angel, 2013)

The City also recommends several publications illustrating typical Bavarian architecture:

- ✘ [Brantl, 1970](#)
- ✘ [Lacher, 1963](#)
- ✘ [Muller, 1971](#)
- ✘ [Siegner, 1965](#)
- ✘ [Siegner, 1970](#)

A more contemporary source is the *Bayern Fachwerk* [Bavarian half-timbered architecture] section of Flickr: <http://flickrhivemind.net/Tags/bayern,fachwerk>

In general, the preference is for authentic, half-timbered building design prior to the 20th century. The Glockenspiel building typifies this architectural style and complies with all City Development Code design elements.

Landscape

Typical Bavarian villages were paved with cobblestones and landscaped with ornamental

plants. Street trees were uncommon, and trees were usually planted in small public spaces and parks. The most common landscape element is the flower box which is located below windows, on balconies and adjacent doorways.

Many Swiss-German plant species are compatible with the Oregonian Willamette Valley climate. These should be selected for their experiential qualities such as color, fragrance, edible, texture... [See [Public Art & Experiential Design](#)]



Windischer's General Blacksmith Shop est. 1905



ENVIRONMENTAL DESIGN

BIOPHILIC DESIGN & URBAN AGRICULTURE



Bavarian villages historically did not incorporate large-scale landscaping such as street trees and greenspaces other than select public spaces and parks. This report proposes to diverge from this theme for two specific reasons: First,

the urban fabric of 19th century Swiss-German villages consisted of narrow streets, building near the street edge and organic block shapes. Mt. Angel has relatively wide streets, setback buildings and a relatively linear block pattern. The addition of greenspace can assist in creating a more human-scale environment. Second, contemporary development recognizes the numerous benefits of introducing natural elements into the built environment. These values include:

- ✘ Biodiversity—urban wildlife
- ✘ Biophilic—connectivity with nature
- ✘ Economic—property value & business attraction
- ✘ Experiential—esthetics, sensory stimulation
- ✘ Health—walkability, air filtration
- ✘ Microclimate—heat island reduction
- ✘ Urban Resiliency—local food

Biophilic Design

Biophilia is the innately emotional affiliation of human beings to other living organisms. Whereas the cities of the Old World were design to keep nature outside the city walls, today's biophilic cities are bringing nature into the city and merging the natural and built environments.

A biophilic city is a city abundant with nature, a city that looks for opportunities to repair and restore and creatively insert nature whenever it

can. It is an outdoor city, a physically active city, in which residents spend time enjoying the biological magic and wonder around them. In biophilic cities, residents care about nature and work on behalf locally and globally. (Beatley, 2011)

Green Streets

A street that uses vegetated facilities to manage stormwater runoff at its source is referred to as a Green Street. A Green Street is a sustainable stormwater strategy that meets regulatory compliance and resource protection goals by using a natural systems approach to manage stormwater, reduce flows, improve water quality and enhance watershed health. (Faha, 2014) There are many opportunities to redesign streets in the downtown to create green streets. These can be incorporated into the Bavarian Theme District through materials and public art. [See [Infrastructure, Resources](#)]

Gardens

There are numerous small urban spaces throughout the downtown and event more opportunities in the streetscape to create gardens. There is a special emphasis on community gardens which may have both floral and edible themes. In addition, the City should consider specialty gardens including:

- ✘ **Bavarian**—Design a garden with traditional Bavarian flowers, vegetables and herbs (depending on climate compatibility).
- ✘ **Biergarten** [Beer Garden]—The area adjacent the historic house on Cleveland Street should become a public space with the potential to serve as a Biergarten. A Biergarten is not just for drinking, and is more often used for family gatherings such as picnics.
- ✘ **Community Garden**—Develop community gardens in multiple locations

ENVIRONMENTAL DESIGN

BIOPHILIC DESIGN & URBAN AGRICULTURE

such as vacant lots, public right-of-way, and others. These may be managed by the City of community organizations.

- ✘ **Decorative/Floral**—Design a formal Bavarian flower garden with geometric symmetry.
- ✘ **Edible**—Plant vegetables, nuts, berries, herbs, spices. Promote community involvement in maintaining and harvesting public space edible landscape.
- ✘ **Medicinal**—Plant traditional or a contemporary garden with medicinal materials.
- ✘ **Meditative/Reflective**—Plant a garden for meditation, reflection and relaxation. Consider aromatic therapy, white noise, animation...
- ✘ Others

Green Roofs

In addition to the values previously mentioned, there are numerous benefits to retrofitting roofs with vegetation in the downtown:

- ✘ Improved insulation
- ✘ Increased public space
- ✘ Less maintenance
- ✘ Longer life-span

Green roofs can be incorporated into the



City Hall with Street Trees

ENVIRONMENTAL DESIGN

BIOPHILIC DESIGN & URBAN AGRICULTURE

Bavarian Theme District with existing flat roof buildings.

Green Walls

Green walls can be attached to building walls or on trellises. They provide the values mentioned and:

- ✘ Improved insulation
- ✘ Reduced glare
- ✘ Reduced noise
- ✘ Visual screening

They can support the Bavarian Theme by creating a green backdrop celebrating traditional Swiss-German development.

Urban Agriculture

Urban agriculture is the practice of cultivating, processing, and distributing food in or around a village, town, or city. (Bailkey, 2000)

There are several avenues for Mt. Angel to take in regards to urban farming:

- ✘ Edible Landscaping
- ✘ Small Urban Spaces
- ✘ Parklets
- ✘ Green Streets
- ✘ Green Roofs
- ✘ Green Walls
- ✘ Community Gardens
- ✘ Vertical Farming

The Biophilic Design & Urban Agriculture Plan illustrates several specific sites, and there are many others infused throughout



Green Wall

ENVIRONMENTAL DESIGN

BIOPHILIC DESIGN & URBAN AGRICULTURE

the downtown. [See [Biophilic Design & Urban Agriculture Plan](#)]

Community gardens are especially vital in providing a multi-purpose greenspace. These may be organized by the City government or run by a community organization. They may be thematic (e.g. floral) or general (e.g. individual choice). They are especially suitable for underused and vacant lots as a temporary use. These gardens can support the Bavarian theme in their appearance, materials and associated events.

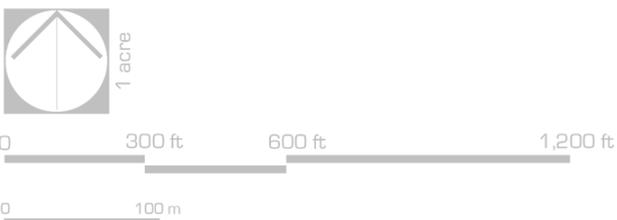
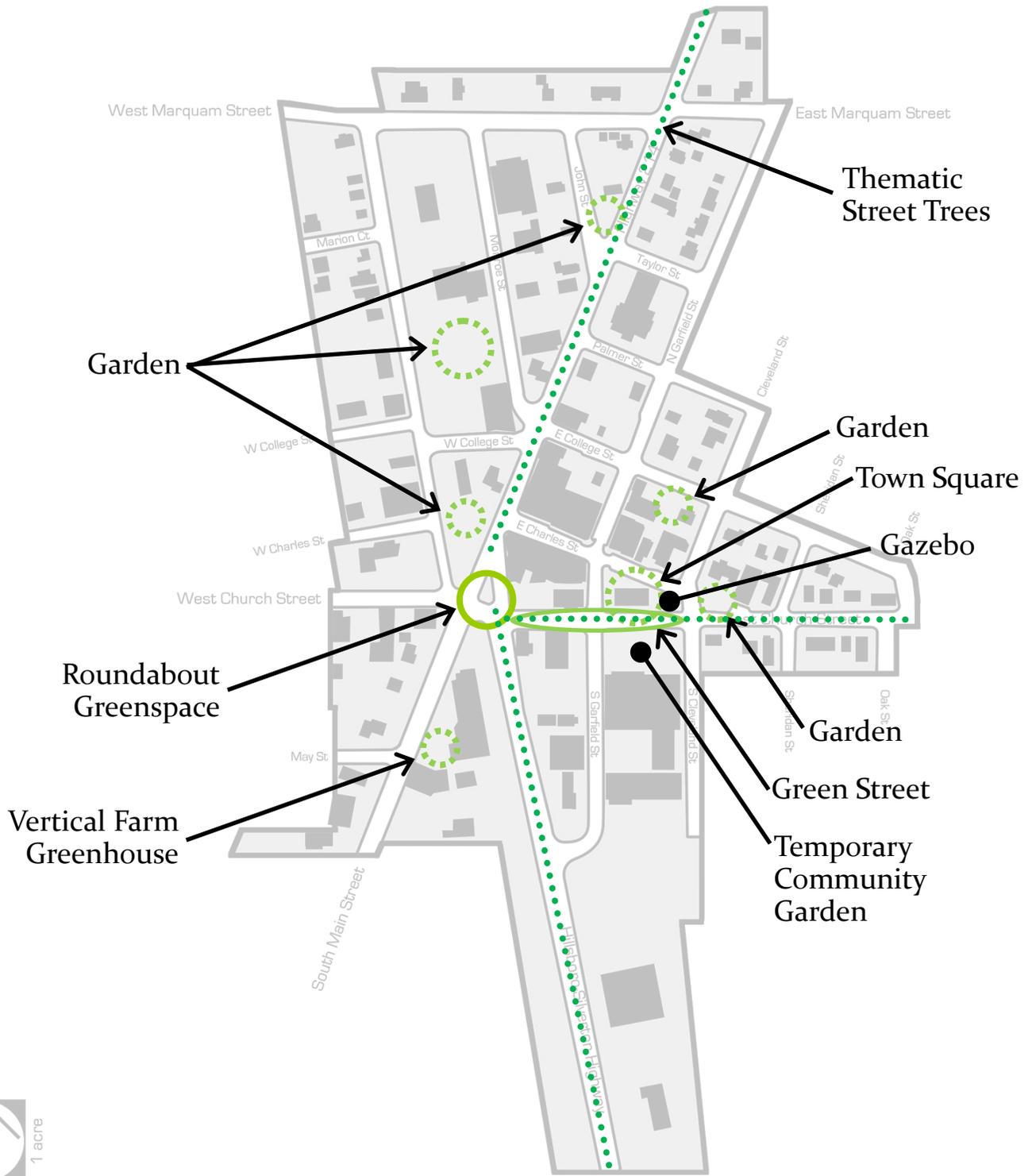
Urban agriculture should be integrated with various community development strategies including agritourism and culinary tourism, the farmers' market, seasonal events, and educational programs.



Community Edible Garden

BAVARIAN THEME DISTRICT

BIOPHILIC DESIGN & URBAN AGRICULTURE



ENVIRONMENTAL DESIGN

PUBLIC ART & EXPERIENTIAL DESIGN



Visual arts contribute to and provide experiences which enrich and better our social and physical environment. The commissioning of art works in public places, in addition to furthering the policy of fostering art and developing

artists, enriches public perception of government buildings, parks, and other public spaces. (City of Eugene, 2014)

Public Art

Mt. Angel has an amazing “stage” for the full spectrum of public art:

- ✘ Monuments & Buildings
- ✘ Statues & Sculptures
- ✘ Public Spaces
- ✘ Greenspaces
- ✘ Earthworks
- ✘ Animated Space/Objects
- ✘ Installations
- ✘ Lighting & Pyrotechnics
- ✘ Laser Art & Holography
- ✘ Computer-Generated Art
- ✘ Photography & Video
- ✘ Murals & Mosaics
- ✘ Functional Elements
- ✘ Interactive Art
- ✘ Social Practice
- ✘ Performance Art
- ✘ Street Entertainment
- ✘ Happenings
- ✘ Smart/Flash Mobs
- ✘ Guerrilla Art
- ✘ Dance & Theater
- ✘ Music & Sounds
- ✘ Parades & Processions
- ✘ Fairs & Festivals

Urban planning for Mt. Angel should include siting locations for public art of all types, and programming to engage the community and implement art projects. These public art forms may also be integrated with other City objectives such as resource management (e.g. using recycled materials for art projects) and energy (e.g. constructing public art that captures solar and/or wind energy).

Experiential Design

Mt. Angel should develop the Edelweiss Village to encompass all senses: sight, sound, smell, taste and touch. Visual arts, music, fragrant landscaping, culinary arts, and water features are a few of the many ways to inject these in the environment. Experiential design not only



Der Maibaum

ENVIRONMENTAL DESIGN

PUBLIC ART & EXPERIENTIAL DESIGN

includes consideration of the senses, but the entire realm of experience:

- ✘ Esthetics
 - ✘ Entertainment
 - ✘ Education
 - ✘ Escapism
- (Pine, 1999)

People can have *transforming* experiences when these four areas are powerful and balanced. For example, a visit to Oktoberfest can be esthetically pleasing and entertaining ('eatertainment'). When accompanied by learning about Bavarian culture and being immersed in the language, music, dance..., it can be transforming.

Esthetics can be enhanced through environmental design and public art. Entertainment can be increased through new events and activities. [See [Culture Events](#)] Education can be developed through kiosks, plaques, online media and educational programs. Escapism can be enhanced through [Bavarian] thematic development and deeper citizen participation.



Music, Flowers & Mural

ENVIRONMENTAL DESIGN

RECOMMENDATIONS

Urban Design & Public Space

38. **COMMUNITY CENTER** – Relocate City Hall and convert the existing building into a community center. [If the building is not to be retained, the site should be converted to a town square] The Community Center can serve as a recycled materials collection and processing site [transfer station]. This service can also coordinate programs related to recycled materials.
39. **DOWNTOWN MAP** – Prepare a walking tour map of downtown trees, flowers, edible landscapes, community gardens, salad tour...
40. **FRONT YARD SPACE**—Encourage activities in residential front yards to increase social connectivity.
41. **PARKLETS**—Provide parklets by converting one or more parking spaces to a temporary or permanent public space. Consider an annual design competition. Convert underused parking into raised bed gardens for seniors.
42. **PUBLIC FLEX-SPACE**—Make small public spaces multi-functional and easily modifiable.
43. **PUBLIC SPACE ELEMENTS**—Consider traditional elements for public space:
- **Capacity**
 - **Food**. Encourage outdoor dining and allow food carts.
 - **Sitting space**. Expand seating opportunities downtown especially benches within view of the Glockenspiel and moveable tables and seats in other areas.
 - **Light**. Balance light and shade for seasonal temperatures. Light pathways, pedestrian areas and public spaces for evening use, nighttime events and safety.
 - **Security**. Use Crime Prevention Through Environmental Design (CPTED) strategies.
 - **Shade**
 - **Streetscape connectivity**
- **Trees**
 - **Water**. Introduce water into the downtown through multi-functional fountains and/or pop jets. These can be turned on or off depending on programming for the public space.
44. **PUBLIC TOILETS**—Modernize the downtown public bathrooms with a skylight, hand dryer, and waterless urinals.
45. **STREET FURNITURE**—Provide essential street furniture:
- **Benches**
 - **Bollards**
 - **Bicycle racks**
 - **Canopies/shelters**
 - **Drinking fountains**
 - **Information kiosk**. Provide a Bavarian kiosk to announce public events and display a downtown map.
 - **Lamp Posts**. Invest in solar street lamps in the Edelweiss Village. Use a style compatible with Bavarian design and also have them be shielded to preserve “dark sky.”
 - **Moveable tables and seats**
 - **Pet waste collection stations**
 - **Recycling bins**
46. **TEMPORARY USES** – Encourage temporary uses relatively easy to permit. These could include community gardens, tot lots, school projects, and community activities on vacant lots.
47. **TOWN SQUARE**—Development a downtown public space that serves as a node and communal center for the city. Relocate City Hall and convert its current site to a town square. This would provide a community gathering place and a focus for the downtown. If the existing City Hall must be retained, it should be converted to a Community Center.

ENVIRONMENTAL DESIGN

RECOMMENDATIONS

Architecture & Landscape

48. AUTHENTICITY & INTEGRITY—Strive for Bavarian-themed authenticity and integrity in terms of design, materials and function.
49. BAVARIAN HERALDIC LOZENGE PATTERN—Incorporate the Bavarian blue and white diamond pattern as a graphic design element. [See [Bavarian Coat of Arms](#)]
50. BAVARIAN THEMED LANDSCAPING – Incorporate plant materials such as hops, grapes, and others connected with Swiss-German culture. The Biergarten could be landscaped with materials integral to making beer such as hops, herbs and berries.
51. BED AND BREAKFAST—Convert one or more historic homes (+50 years old) to a “bed and breakfast” [Übernachtung mit Frühstück].
52. DARK SKY & SCIENCE FACTORY OBSERVATORY – Mt. Angel could adopt lighting policies that preserve dark skies and enable star viewing both for tourism and a potential temporary or permanent research station.
53. DESIGN GUIDELINES—Develop design guidelines for Bavarian-themed street furniture, signage, façades, lighting and landscaping. These should coordinate with and unify the Bavarian Theme District Development code.
54. EDIBLE LANDSCAPING—Plant fruit and nut trees, berry bushes and herb gardens throughout the city.
55. EDUCATIONAL LANDSCAPE—Provide plaques or markers to identify and describe plant materials, historic buildings and public spaces.
56. FESTHALL PROGRAMMING ENTRY STATEMENTS—Create entry statements for the City at key locations. Expand the use of the Festhall through Mt. Angel Community Foundation programming with organizations such as the schools, Chamber of Commerce...
57. GAZEBO – Revitalize the downtown gazebo for music, dance, culinary festivals, etc.
58. GLOCKENSPIEL – Promote the Glockenspiel through events and activities such as a photography contest.
59. HANGING SIGNS AND FLOWER BASKETS – Encourage traditional Bavarian hanging signs and hanging flower baskets either separately or combined.
60. IMPLEMENT MAINTENANCE SCHEDULES—Prevent the “broken windows” effect by establishing regular schedules for maintaining buildings and public spaces.
61. NATIVE SPECIES – Integrate native plants like vine maple, Oregon grape, and wild ginger into public spaces.
62. PLANT MATERIALS—Plant nitrogen fixers, air-purifying flora, colorful, native species, edible species and perennials. Plant annuals and non-natives only in controlled environments such as garden beds, green roofs, green walls, etc.
63. VERTICAL DIVERSITY—Consider various growth layers. I.e. groundcover, understory, canopy.
64. VISIBILITY—Encourage picket fences, wrought-iron and low vegetation (<3’ high) to encourage visibility for social engagement and safety. Do not allow barbed tape, razor wire or similar security fencing.
65. WAYFINDING & SIGNAGE—Provide signage to show walking and cycling routes that connect to various City places, trail systems and nearby places such as the Mt. Angel Abbey.
66. WINDOW FLOWER BOXES – Window box planters should be used throughout the downtown. Contests and incentives should be created to help beautify and establish sense of place.

ENVIRONMENTAL DESIGN

RECOMMENDATIONS

Biophilic Design & Urban Agriculture

67. **AGRICULTURAL BUSINESS INCUBATOR**—Provide resources to introduce agricultural businesses in the downtown. E.g. an underused lot or abandoned structure.
68. **FARMERS MARKET**—Organize a seasonal farmers' market to be held monthly on a Saturday in the downtown. This could be combined with other events such as an art fair or culinary festival. The Mt. Telephone Co. parking lot or Charles Street are ideal locations for this event.
69. **GARDENS**—Vary the types of gardens proposed for temporary and permanent locations. [See *Biophilic Design & Urban Agriculture*]
GREEN ROOFS—Redesign roofs to accommodate vegetation to inhibit heat island effect, improve insulation, provide esthetics, promote urban wildlife, provide edible landscaping and others.
70. **GREEN STREETS**—Redesign roads to incorporate bioswales and rainwater gardens to improve stormwater management, enhance walkability and provide esthetics. (Metro, 2002)
71. **GREEN WALL / VERTICAL GARDEN**—There are multiple sites in the city that are conducive to green walls or vertical gardens. These can be decorative, vegetable, herb or a combination of these types. The Performing Arts Center, Pubic Library, abandoned gas station and Mt. Angel Historic Society are good candidates for green walls with vines or a trellis system.
72. **GREENHOUSE**—Develop a community greenhouse with raised beds, aquaponics, and other emerging technologies. Consider listed garden types and others.
73. **LANDSCAPE MATERIALS:**
Street Trees. Plant street trees that are

suitable to this climate zone and meet environmental design objectives: shade canopy, Bavarian theme, nature-friendly, experiential, fragrant, edible...

Nature-Friendly Design. Develop public spaces that support urban wildlife such as birds and squirrels.

Experiential. Plant species that stimulate all senses: sight, sound, smell, taste and touch.

Fragrant Vegetation. Incorporate vegetation that has seasonal fragrance especially at the intersections of East College and Cleveland Streets; and East College and Garfield Streets to take advantage of the predominantly northern wind patterns.

Edible. Plant edible landscape materials along pedestrian paths and bikeways.

74. **MARKETPLACE** – Partner with a market to provide fresh produce. Refurbish or replace the abandoned building adjacent the Fire Station with a commercial or community market.
75. **SALAD TOUR**—Create a walking route through the downtown which highlights various vegetables and herbs at different locations.
76. **SEASONAL PLANTING AND FIX-UP DAYS**—Select spring and autumn dates for a community effort to clean the downtown and replant the flowerboxes, gardens, greenspaces...
77. **SEED SWAP**—Organize a seed swap for citizens to share seeds and plant materials. This could be coordinated with local businesses and farms.

Public Art & Experiential Design

78. **ART FAIRS**—Host art fairs, contents, festivals and other events.
79. **ARTS & CRAFTS PROGRAMS**—Create community classes and workshops.

ENVIRONMENTAL DESIGN

RECOMMENDATIONS

80. **BANNERS & FLAGS**—Place banners and flags on lamp posts, flag posts and overhanging wires to promote seasonal events and activities.
81. **COLEGIO CESAR CHAVEZ MONUMENT**—Erect a monument commemorating the Chicano movement with the Colegio Cesar Chavez.
82. **COWS ON PARADE**—Organize a variation on the “Cows on Parade” public art program. For example a “Hide-n-Seek Stein” game would locate oversized Steins throughout the downtown for tourists to find while touring the City. More information on this international public art program is at Cow Parade <http://www.cowparade.com/>.
83. **LANDSCAPE COMPETITIONS**—Create competitions for flower boxes, hanging flower baskets, floral groundcover and other landscape elements.
84. **MURALS**—Select sites for temporary or permanent murals such as the grain elevator...
85. **PLEIN AIR PAINT OUTSIDE COMPETITION**—Host an outdoor painting competition.
86. **PUBLIC ART** – Introduce public art throughout the downtown via statues, street art, murals and others. Several sites should be developed to accommodate rotational art installations. The City Walk should be designed in conjunction with the downtown public art.
87. **SEASONAL ART**—Provide venues for rotating art installations and murals.
88. **STREET ART**—Provide opportunities for temporary street art such as murals, 3D street painting, graffiti, installations and others. These public art elements can be combined with special events. [See [Culture, Events](#)]

INFRASTRUCTURE

WALKABILITY



Walkability reflects overall walking conditions in an area. Walkability takes into account the quality of pedestrian facilities, roadway conditions, land use patterns, community support, security and comfort for walking. Walkability can

be evaluated at various scales. At a site scale, walkability is affected by the quality of pathways, building accessways and related facilities. At a street or neighborhood level, it is affected by the existence of sidewalks and crosswalks, and roadway conditions (road widths, traffic volumes and speeds). At the

community level it is also affected by land use accessibility, such as the relative location of common destinations and the quality of connections between them. (TDM Encyclopedia, 2014)

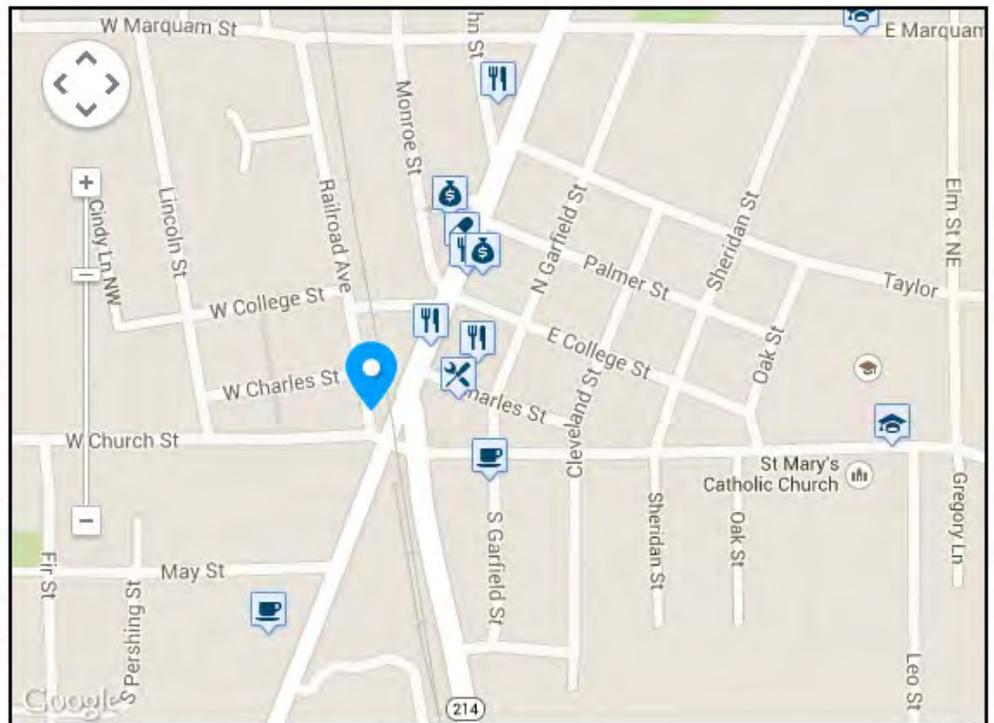


Shared Space Sign

Walk Score is a website that measures walkability and scores locations based on various measures. Scores range from 0 to 100 with 100 being the best walkability. The Walk Score algorithm awards points based on the distance to the closest amenity in each category. If the closest amenity in a category is within .25 miles (or .4 km), we assign the

Restaurants:	
Happy Garden Inc	.09mi
Coffee:	
Mt Angel Sausage Co	.09mi
Bars:	
Milltown Pub	3.6mi
Groceries:	
Monitor Market & Deli	3.5mi
Parks:	
Fisher Memorial Park	.2mi
Schools:	
St Marys Public School	.3mi
Shopping:	
Touch of Bavaria	.7mi
Entertainment:	
Palace Theatre	4.3mi
Errands:	
Bochsler True Value Hardware	.08mi

WalkScore 60
Somewhat Walkable



WalkScore

INFRASTRUCTURE

WALKABILITY

maximum number of points. The number of points declines as the distance approaches 1 mile (or 1.6 km)—no points are awarded for amenities further than 1 mile. Each category is weighted equally and the points are summed and normalized to yield a score from 0–100. The number of nearby amenities is the leading predictor of whether people walk.” Relevant amenities include “businesses, parks, theaters, schools and other common destinations.” (Walk Score, 2014)

Mt. Angel scores 60 out of 100 which is considered “somewhat walkable so some errands can be accomplished on foot.”

Downtown Mt. Angel can become more walkable by improving sidewalks and intersections; enhancing pedestrian safety; traffic calming; and increasing the number of amenities.



Zebra Crossings & Traffic Circle

INFRASTRUCTURE

MULTI-MODAL TRANSPORTATION



The Mt. Angel downtown focuses on auto-dependent design consistent with similar rural towns throughout the United States.

Shifting to a multi-modal transportation strategy of supporting

and encouraging alternative modes of transportation will create a more vibrant, resilient and sustainable downtown.

Multi-modal strategies include:

- ✘ Intermodal Transportation
- ✘ Walkability
- ✘ Shared Space
- ✘ Bicycle-Friendly Design
- ✘ Streetscape Design
- ✘ Parking Management
- ✘ Public Transit

The unifying principle behind these is to consider transportation from a systems perspective. Specifically, how can we integrate various forms of transportation to be the most efficient, safest and most appropriate for downtown Mt. Angel? The immediate focus should be to make the streetscape multi-functional with equal emphasis on pedestrians, cyclists and drivers. Streets that balance these interests equally are “shared spaces.” This is a common concept in Bavaria and Europe, but not as easily introduced to the United States.

Mt. Angel can take a leadership position in promoting this community development approach. An ideal candidate for this concept is Charles Street specifically

between Garfield and Cleveland Streets. As this program becomes successful, it may be expanded. This particular section of street may even be temporarily or permanently closed as a



Shared Space Street

“festival street” or part of a revitalized town square.

Intermodal Transportation

In addition to become more multi-modal, Mt. Angel should consider how to become intermodal: how to easily shift from one mode

INFRASTRUCTURE

MULTI-MODAL TRANSPORTATION

to another. Bicycle racks located throughout the downtown implement this approach. A small “park & ride” site will support intermodal transportation as transit grows.

The previous section outlined the importance of walkability and shared space.

Bicycle-friendly Design

Bicycle-friendly design complements these components for both local connectivity, recreation and tourism. Mt. Angel can become more bicycle friendly with the following:

- ✘ Wayfinding
- ✘ Traffic Calming
- ✘ Traffic Reduction
- ✘ Intersection Treatment
- ✘ Bike Lanes
- ✘ Bike Racks & Stations

[See [Local Bicycle Tour](#)]

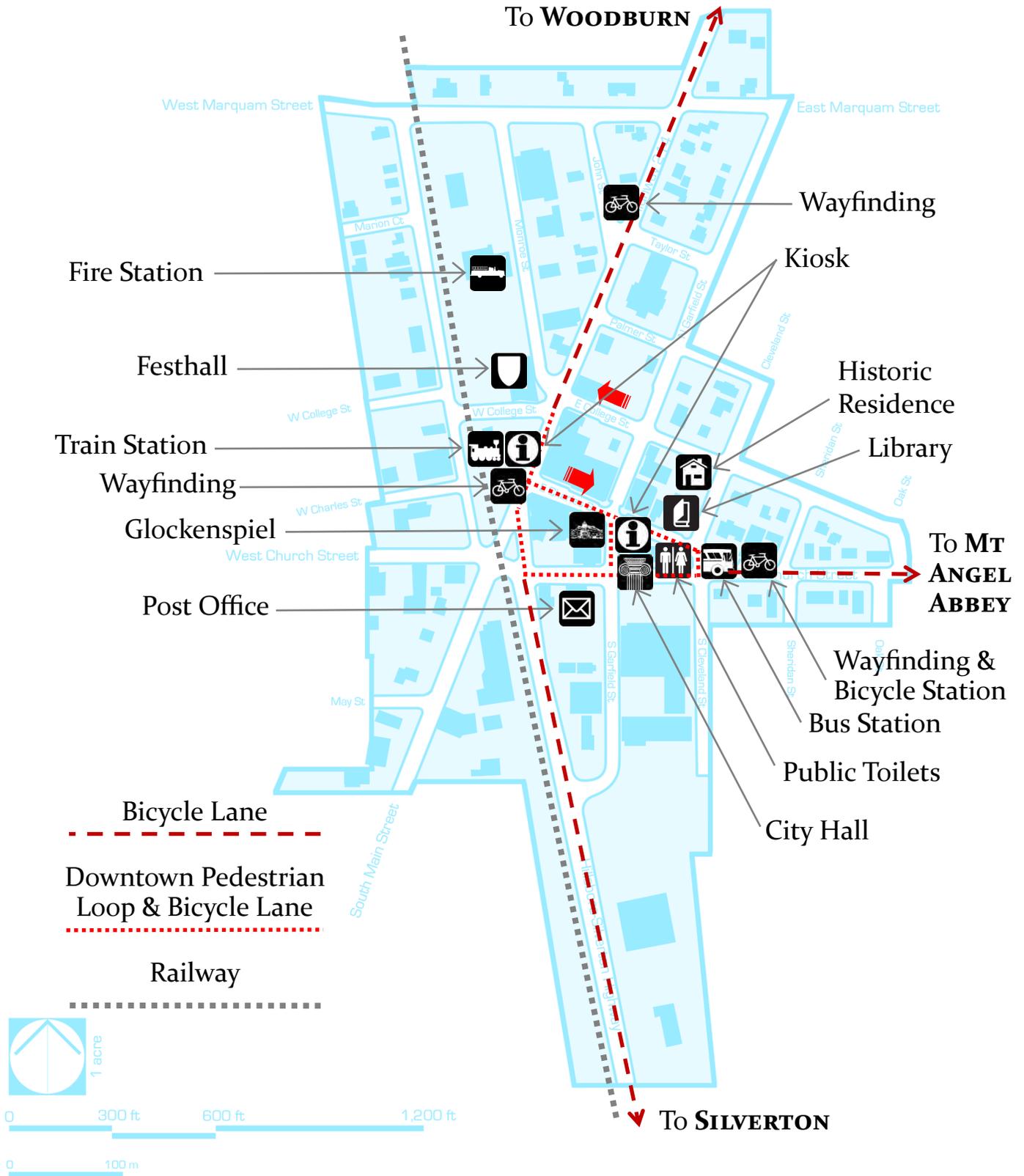
As the City of Mt. Angel grows, parking demand management and transit programs should be incorporated in the transportation systems plan.



Bike Lanes & Parklet

BAVARIAN THEME DISTRICT

MULTI-MODAL TRANSPORTATION



INFRASTRUCTURE RESOURCES



Systems approaches to resource and energy conservation include ecodistricts and eco-industrial networks.

EcoDistrict

Mt. Angel should adopt an accelerated district-scale sustainability approach for the

downtown: a *Bavarian-Theme EcoDistrict*.

In an EcoDistrict, the community is integrated so that energy and resources are used efficiently and equitably. Information and guides for forming these sustainable district programs are available through [EcoDistricts](#).

Eco-Industrial Networks

Eco-industrial networks originated in the exchange of resources between various industries that were collocated or in close enough proximity to benefit from sharing materials and energy. This concept is related to **Industrial symbiosis**, the sharing of services, utility, and by-product resources among industries in order to add value, reduce costs and improve the environment. (Agarwal 2008) Eco-industrial networks view production as a circular rather than linear economic system. In this viewpoint, “waste” is considered another resource, and businesses actively collaborate to share all resources. The eco-industrial network model is also applicable to agricultural areas and commercial districts.

These types of business-to-business (B2B) networks can be facilitated through the City, Chamber of Commerce, or downtown business association.

Reduce, Reuse, Recycle

The Edelweiss Village can adopt a variety of programs to accelerate reducing, reusing and recycling. Specific examples include banning single-use plastic bags, providing dispersed, labeled recycle bins, on-site trash sorting...

Water Management

The City of Mt. Angel should also adopt a systems approach to water management. Much of downtown Mt. Angel is covered in impermeable asphalt and concrete. In conjunction with other programs (e.g. streetscape redesign), the City should begin shifting to open landscaping and impermeable surfaces:

- ✘ Bioretention Areas
- ✘ Bioswales



Oak Barrel Trash Bin

INFRASTRUCTURE RESOURCES

- ✘ Gravel
- ✘ Green Roofs
- ✘ Mulched Beds
- ✘ Spaced and/or Open Pavers
- ✘ Impermeable Pavers
- ✘ Impermeable Concrete
- ✘ Planting Beds
- ✘ Rain Barrels & Cisterns
- ✘ Rainwater Gardens
- ✘ Stone Aggregate
- ✘ Turf (with recycled plastic grid)



Green Street Rain Garden

INFRASTRUCTURE

ENERGY



Renewable energy is relatively easy to incorporate in the urban fabric from a design consideration. It is much more complex to regulate, integrate into the power grid, and gain public acceptance. The City of Mt. Angel can take a leadership

position by adopting renewable energy for municipal structures and projects. The downtown can take a similar leadership role by incorporating alternative energy sources for commercial activities.

Solar Energy

Photovoltaic (PV) panels can be installed in the City Hall rooftop without impacting the Bavarian theme design. The initial costs will be rewarded with long-term savings, municipal efficiency and civic pride. For future development, passive solar design should be incorporated to reduce or eliminate energy demands for heating and cooling.

The library and gazebo are additional sites to be considered for solar panels. If the panels are too obtrusive, solar sculptures can be designed as a form of collaborative design for public art. An example is the solar flower which could be shaped similar to an Edelweiss.

Wind Energy

Small scale wind energy conversion systems (WECS) can be easily mounted on rooftops or small sites. Again, City Hall should initiate a small WECS project to supplement the power needs of building operations.

Wind energy also lends itself to public art designs. For example, the Maibaum could be

designed so that the business figures rotate in the wind generating power and adding to the visual interest of the pole. [See [Public Art & Experiential Design](#)]

Alternative Fuels

When the City replaces or adds to its motor pool, new vehicles should operate on alternative fuels, hybrids or participate in a sustainability program.



Solar Flower

German-Engineered Sustainability

The City may even wish to collaborate with Swiss-German cities that are making dramatic changes in practices to comply with federal

INFRASTRUCTURE

ENERGY

laws to reduce ecological impacts and mitigate global climate change.

As the City continues to grow, other opportunities for renewable energy should be incorporated in operations and maintenance.



Wind Energy & Alternative Fuels

INFRASTRUCTURE

RECOMMENDATIONS

Walkability

- 89. **FESTIVAL STREET**—Create a festival street by temporary or permanent closure of a street. Charles Street is an ideal location for this venue for public events and activities.
- 90. **MULTI-MODAL STREETS**—Design streets to accommodate cars, public transit, cyclists and pedestrians.
- 91. **MULTI-PURPOSE ALLEYWAYS**—Activate underused alleys such as the one between East College Street and Charles Street. These can be redesigned to appear like narrow Bavarian streets.

Multi-Modal Transportation

- 92. **BEER TRAIN**—Create a short-run service connecting Oktoberfest tourists with nearby cities.
- 93. **BICYCLE REPAIR STATION**—Install a repair station in the downtown along the promoted tour route such as the intersection of Main Street and Church Street.
- 94. **BICYCLE TOURISM**—Promote cycle tourism with a dedicated bike lane, signage and tour map. Lobby to have the bicycle route through town part of the Oregon Scenic Bikeway.
- 95. **BIKE LANES/TRAILS**—Delineate and sign bike lanes through town connecting to the Intertwine and Mt. Angel Abbey. Include multiple bike parking/rack sites.
- 96. **BIKE TOUR**—Create a long-distance bike touring path that runs from Woodburn through Mt. Angel and Silverton and terminates in Salem. Include a downtown loop that connects to the Mt. Angel Abbey.
- 97. **CAR-FREE DAY AND BIKE DAY**—Organize an annual event to close one or more downtown streets and open them to cyclists, rollerbladers, skateboards, pedestrians... Charles Street is a great candidate for this event.

- 98. **CITY WALK**—Design a pedestrian loop through the downtown with signage that includes a map, features, history and distances. Scheduled tours can be conducted during seasonal events.
- 99. **DESIRE LINES**—Design for actual routes (e.g. across corners and through lots). These “desire lines” should match walkway and bikeway alignments.
- 100. **LIGHT RAIL**—When funding becomes available, establish a low speed passenger rail service.
- 101. **ONE-WAY COUPLET**—Redesign Charles Street as a one-way east-bound street and College Street one-way west-bound.
- 102. **PARKING AND TRANSPORTATION DEMAND MANAGEMENT**—Create a Parking and Transportation Demand Management Plan to evaluate streetscape design and parking needs.
- 103. **ROUNDAABOUTS & TRAFFIC CIRCLES**—Design roundabouts and traffic circles for traffic calming and Bavarian Theme. The intersection of Highway 214 and Church Street is an ideal location for a roundabout. The intersections of Charles and Church with Garfield are recommended for traffic circles. [See [Urban Design and Public Space Plan](#)]
- 104. **SAFE ROUTES TO SCHOOL**—Incorporate this tool when redesigning streets for walkability and bike friendliness.
- 105. **SHARED SPACES**—Identify one or more downtown streets as “shared spaces” similar to the Dutch Woonerfs. Use the European sign for shared space. [See [Shared Space Sign](#)]
- 106. **SKINNY STREETS**—Use the “road diet” to reduce street widths. Use the non-vehicle right-of-way for bike lanes, greenspace, gardens, bioswales...
- 107. **TRAFFIC CALMING**—Incorporate traffic calming strategies:

INFRASTRUCTURE

RECOMMENDATIONS

Couplets

Crosswalk & intersection treatments

Curb extensions

Medians & islands

Pavement treatments

Shared space

Signage

Skinny streets

Traffic circles & roundabouts

108. WAYFINDING—Provide signage showing bicycle and pedestrian tour routes, distances to destinations, the bicycle repair station, bathrooms...

117. SOLAR ENERGY FOUNTAIN—Convert the Oktoberfest Joy fountain to solar power.
118. SOLAR FLOWER—Install “solar flowers” to combine art and renewable energy. Consider locating near the Train Station.
119. SOLAR PANELS—Provide incentives to install solar panels on commercial, office and multi-family residential development in the downtown.
120. SOLAR SUN BOXES—Install solar sun boxes, small solar powered acoustic speakers that play ambience inducing music.



Resources

109. LOW IMPACT DEVELOPMENT—Use LID practices in conjunction with green street development.
110. PERMEABLE SURFACES—Replace concrete and asphalt surfaces with permeable pavers or pavement.
111. RAINWATER HARVESTING—Develop and implement a rainwater harvesting program for the downtown.
112. RECYCLABLE ART PROJECTS—Community projects to create large-scale public art with recycled materials.
113. RECYCLED AND/OR BIODEGRADABLE PLAY STRUCTURE—Construct an interactive play structure from inexpensive, biodegradable materials.

Energy

114. ENERGY CONSERVATION MURAL—Promote energy awareness through a downtown mural.
115. MAY POLE WIND ENERGY—Alter the features on the May Pole to rotate and create wind power.
116. REGENERATIVE SYSTEMS DESIGN WITH ART—Organize a design competition related to combining recycled materials, renewable energy and public art.

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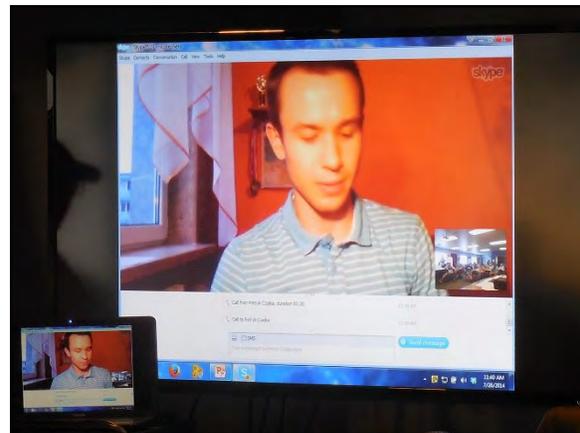
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Kit Stephens & UAV

Editing, mapping, aerial photography and photosimulations by [Stephens Planning & Design LLC](http://StephensPlanning.com)





Community Planning Event

A carefully structured collaborative event at which all stakeholders, including the local community, work closely with independent specialists from all relevant disciplines to make plans for the future of that community or some aspect of it.

Nick Waters

Design Charrette

A design charrette is a [single or] multiple-day collaborative design and planning workshop held on-site and inclusive of all affected stakeholders.

National Charrette Institute

City of Mt Angel Charrette

The purpose of the design charrette will be to identify specific spatial planning and urban design issues and solutions: downtown revitalization, public space, Bavarian theming and tourism development.

Schedule Outline

July 26 – Design Charrette
Conduct the event at the City Fire Hall with invited attendees.

September – City Council and/or Planning Commission
Present recommendations, report and multimedia.

Requirements

- Venue (City Fire Hall)
- Materials (flip charts, maps, sketchpads, markers...)
- Food and refreshment
- LCD projector and screen

Design Team

- Local Officials & Citizens
- Oregon Department of Land Development and Conservation Representatives
- University of Oregon “Green Cities” Students
- Gdansk University of Technology (GUT) “Public Space” Students

Preliminary Program

July 26, 2014

- 8:30-9:00 Registration
- 9:00-10:00 Introductions
- 10:00-11:00 Downtown Tour
- 11:00-12:00 GUT Teleconference
- 12:00-12:45 Lunch
- 12:45-1:00 Group Photo
- 1:00-2:30 Design
- 2:30-2:45 Break/Refreshments
- 2:45-3:15 Prioritization
- 3:15-3:45 Design Refinement
- 3:45-4:15 Presentations
- 4:15-4:30 Closing Remarks

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UNIVERSITY OF OREGON



GDAŃSK UNIVERSITY OF TECHNOLOGY

University of Oregon “Green Cities”

The Green Cities course examines the history and future of the interface between urban growth and environmental concerns, and the technological, social, and political forces that continue to shape it.
ppgm.uoregon.edu

Gdansk University of Technology “Public Space”

The uniqueness of the student research club LEM-ur initiative, in cooperation with ISOCARP, relies on an innovative method of work preparation. The main idea is to join together the efforts of the students, who are open to new experiences and willing to cooperate with the experience of internationally recognized practitioners and theorists in the field of urban design and planning.
www.pg.gda.pl/en/index.php/faculties/ava

Preliminary Program July 26, 2014

8:30-9:00 Registration and refreshments at the Fire Hall.

9:00-10:00 Greetings, Mayor Andrew Otte. Background, City Administrator Eileen Stein. Community Development, DLCD Economic Analyst Tom Hogue, Program, DLCD Rep Angela Lazarean & University Instructor/Consultant Ric Stephens.

10:00-11:00 Downtown tour conducted by local citizens.

11:00-12:00 Public Space Teleconference (LEM-UR, Gdansk, Poland)

12:00-12:45 Lunch at the Community Center

12:45-1:00 Group photo taken by an unmanned aerial vehicle.

1:00-2:30 Design groups will focus on specific topics: 1) downtown revitalization, 2) public space, 3) Bavarian theme, 4) tourism development, 5) public art and 6) programming.

2:30-2:45 Refreshments break.

2:45-3:15 Individuals will prioritize concepts with adhesive dots. (5' per group topic)

3:15-3:45 Design groups will refine their notes and sketches.

3:45-4:15 Each design group will share their key conclusions and recommendations. (5' each)

4:15-4:30 Mayor Otte and/or City Administrator Stein will share closing remarks.

September 18, 2014 Planning Commission Presentation

Researchers, Presenters & Facilitators

University of Oregon

- Leilani Aidana
- Nathan Alexander
- Lauren Beale
- Amber Brazil
- Kayla Byrne
- Tyler Chaffee
- Sydney Clagett
- Thomas Fiorelli
- Erin Fox
- Hannah Fuller
- Richard Garrod
- Nicole Guselli
- Richard Harris
- Michael Hunter
- Frederick Hutchison
- Elena Krasnow
- Andrew Mohr
- Calvin Nguyen
- Megan Ott
- Daniel Platt
- Samantha Priesner
- Jordan Rechia

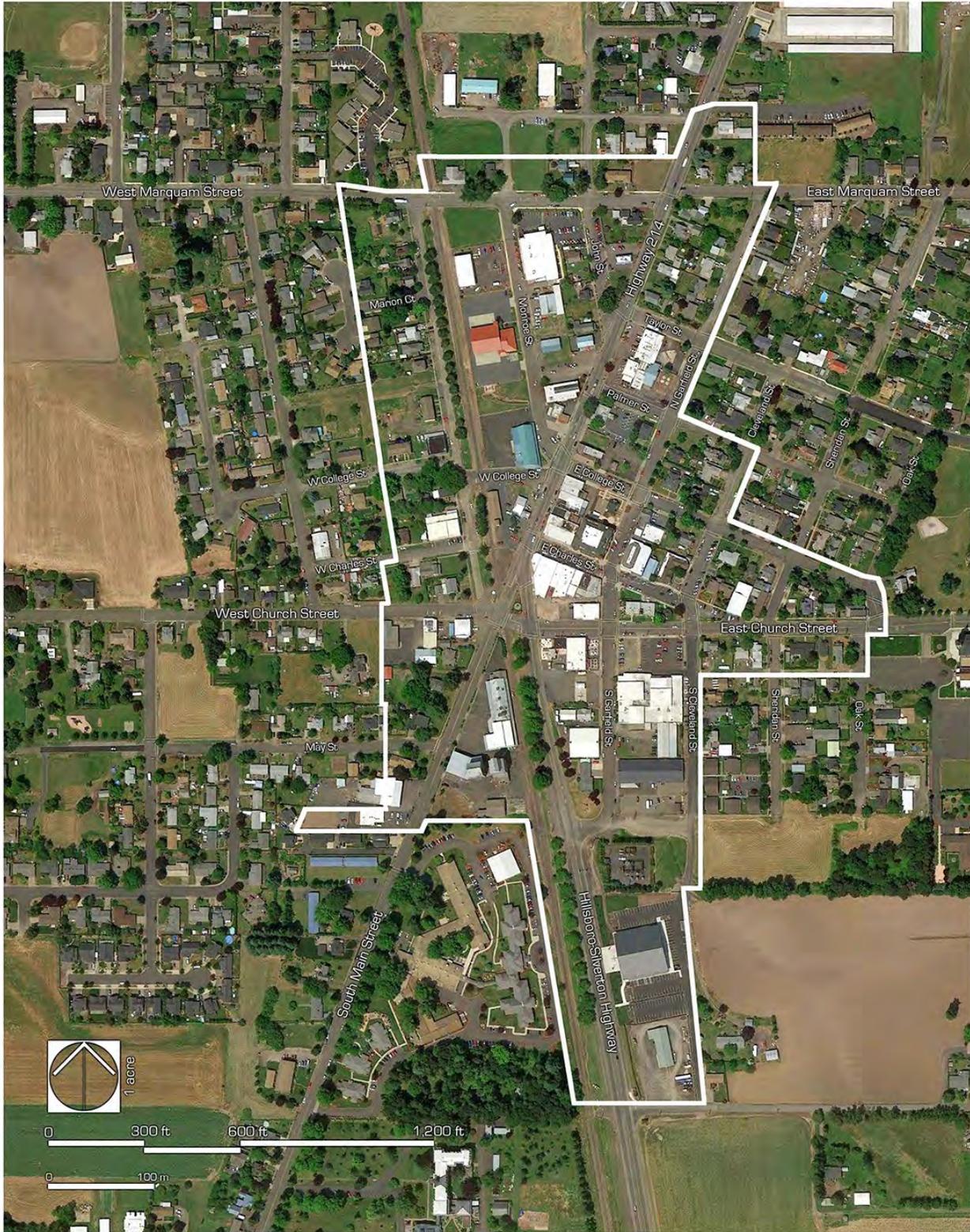
- Jordan Shamash
- Dale Stoneburg
- Stephanie Tabbian
- Willen VanKoon
- Jessica Van Bargaen
- Jacob Woods
- Seamus Yeo
- Taylor Ziolkowski

Gdansk University of Technology

- Hanna “Hania” Obracht
- Prondzyska
- Marta Rusin
- Robertka “Robt” Marta
- Jessika Kreps
- Alina Gredzicka
- Patryk Czajka
- Malgorzata “Gosia” Potocka

City of Mt. Angel

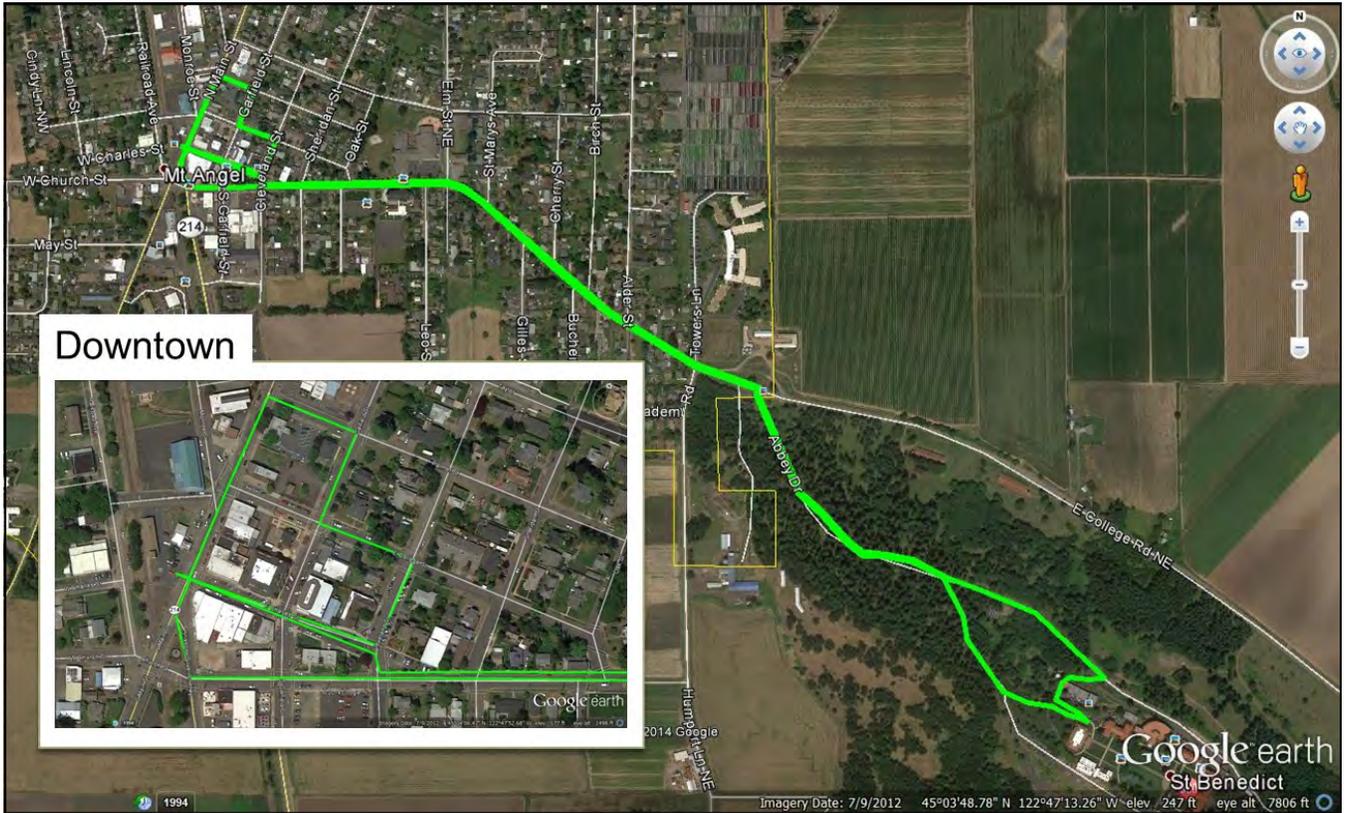
Bavarian Theme District



UNIVERSITY OF OREGON

Planning, Public Policy and Management Department
Summer 2014 "Green Cities"

LOCAL BICYCLE TOUR



INDEX

agriculture.....	19	meditative garden	31
agritourism	19	Mt. Angel Abbey.....	10
alternative fuels	48	multi-modal transportation.....	43
architecture.....	13, 28	National Register of Historic Places.....	10
Bavarian Theme District	12	Oktoberfest.....	14
bicycle-friendly Design	44	Oktoberfest Joy [statue].....	25
bicycle repair station	450	public art.....	35
Biergarten	30	public space	25, 37
biophilic design	30	reduce, reuse, recycle	46
blackletter	14, 15	resources	46
Catholicism.....	10	sense of place.....	24
coat of arms	14	signage	13
Colegio César Chávez.....	17	solar energy.....	48
community garden	30	solar flower	48
culinary tourism	19	St. Mary Church	10
demographic shift	18	street furniture	37
development pattern.....	24	town square	25
downtown plaza	25	traffic calming.....	50
eatertainment	36	United Farm Workers	17
EcoDistrict	46	urban agriculture.....	32
Edelweiss Village	12	walkability	41
edible garden	31	water management.....	46
energy.....	48	wind energy	48
Engelberg.....	10	zebra crossing.....	25
events	20		
experience.....	36		
experiential design	35		
Fachwerk.....	29		
gardens.....	30		
Germanic culture.....	12		
Glockenspiel	13		
green roofs	31		
green streets.....	30		
green walls	32		
Hispanic culture	17		
Indians	10		
intermodal transportation	43		
landscape	29, 39		
Latino culture	18		
lettering.....	15		
Maibaum.....	35		
marketplace	39		
medicinal garden.....	31		

MT. ANGEL BAVARIAN DISTRICT DESIGN CHARRETTE



Design Charrette Participants
Mt. Angel Fire Station, July 26, 2014

STAKEHOLDER INTERVIEWS

MT. ANGEL CODE ASSESSMENT PROJECT

APG conducted interviews with a total of six stakeholders (two interviews were by phone). Stakeholders consisted of:

- Former city planner (contract planner at COG)
- Former Assistant to the City Manager
- Local builders
- Octoberfest committee members
- City engineer

The following table is an overview of how stakeholders responded to each question. Comments are presented in summary form and not attributed to specific stakeholders.

Summary of Stakeholder Interviews
<p>1. What experience(s) do you have working with the Mt. Angel Development Code and development review process?</p> <p>As noted above, stakeholders had a range of experiences with the code.</p>
<p>2. Based on your experiences, what are some of the challenges and problems that could be improved?</p> <ul style="list-style-type: none"> ▪ Code contains conflicting/inconsistent language, causes confusion and inefficiencies ▪ Terminology in code is outdated or not well defined ▪ The overall development fees are too high, the COG planners and city engineers spend too much time reviewing applications. The deposit system should only be used for larger, more complex projects. Set a standard fee for projects where appropriate. <i>Note: it was mentioned that the Council has recently updated the fee structure.</i> ▪ Certain measurements (building height, for example) are not clearly explained - code needs to demonstrate how certain standards are measured ▪ Notice and application processing timelines are not clear ▪ Different standards within the Infill Overlay vs. residential properties outside the overlay causes additional work for adjusting home plans.
<p>3. Are there any specific areas of the code you think should be changed?</p> <ul style="list-style-type: none"> ▪ Infill Overlay Zone contains standards for single-family homes that are too prescriptive: <ul style="list-style-type: none"> – Garage setback standard difficult to meet, many home drawings do not meet the standard so requires a customization to comply with code. A standard allowing the garage to be flush with the home would be more acceptable. – Porch standard is too specific – Standards in the overlay also conflict with standards in other areas of the code – Boundary for the overlay does not make sense - it covers almost the entire city limits. ▪ Backyard and corner lot setback standards are too large, makes it difficult to configure a site with a single family home ▪ Review process in Chapter 2 should be consistent in terms of timelines for notifications and the 120 versus 180 day decision timeline.

- Bavarian theme needs more flexibility for the Design Review Board to approve designs that meet the intent of the standards. Bavarian theme code also needs to be revised to remove the outdated publications that are referenced. This section would benefit from some graphics to help people visualize the intent.
- Sign code contains overly specific font requirements for the Bavarian theme that are difficult to meet and require customization.
- The code language regarding the Design Review Board is not consistent with current practice (code states that the Board will review single family development)
- Subdivision ordinance should be incorporated into development code and conflicting language should be resolved:
 - ROW widths listed in subdivision code do not match those listed in the development code
 - ROW widths do not reflect the true width required - need to include sidewalks, planter strip, etc
 - Planter strip requirement (also found in the Infill Overlay) should be revisited, may not be appropriate in all areas. Maintenance responsibility for planter strips is unclear (city or property owner?) and planter strips may interfere with water lines.
 - Half-street improvement should be defined and match with Public Works standards document (proposed new language).
- Lot size requirements should allow lots smaller than 7,000 s.f. (allow a variance?). Higher densities with more affordable housing are going to be needed in the future.

4. Are there any parts of the code that you think work well?

- Partition process seems to be simple, fees reasonable
- Single family development outside the Overlay has been straightforward
- Bavarian Theme with Design Review Board process generally works well and is quick

5. If you could change just one thing about the code, what would it be?

- The Infill Overlay zone should be removed or revised to be less prescriptive and eliminate conflicts with other parts of code
- Overall development fees need to be reduced, limit the review time for COG planner and city engineer to keep costs lower